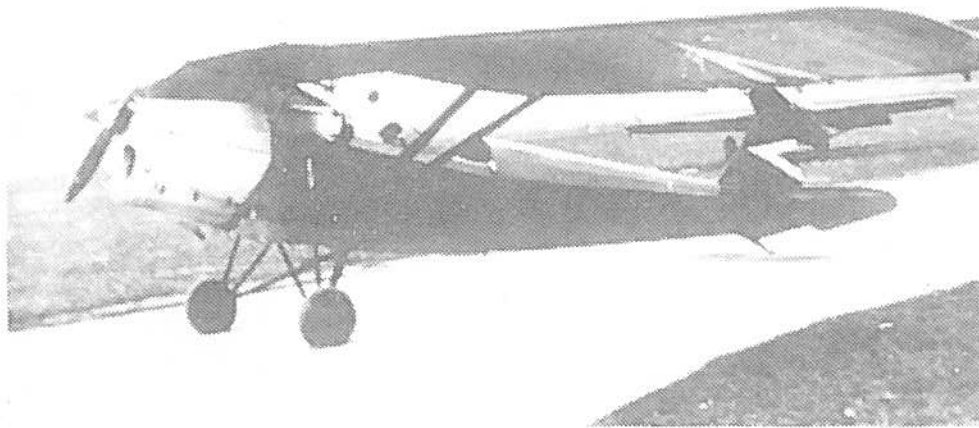


small air forces observer



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RANDOM THOUGHTS (IPMS-CANADA): July '77. \$1.00.

MILITARY JOURNAL #11. \$1.00.

MODELERS' MONTHLY (Singapore) Nov 82 & Jan/Feb 83. 50¢ each.

IL NOTIZIANO (IPMS ITALY) #1 1983 (see abstracts). \$2.00.

LA AVIACION LEGIONAIRIA, Emiliani, et al. \$10.00.

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REGULARNE JEDNOSTKI WOJSKA POLSKIEGO (LOTNICTWO), Kolinski. (hardbound) \$8.00.

LETECTVI + KOSMONAUTIKA #8 - #11 1983. Four-part "Monografie" on Aero L-39 Albatros including scale 3-view drawing and color side-view drawings of Syrian, Cuban, Rumanian, Libyan, Iraqi, Afghanistani, and Vietnamese aircraft. As a set only. \$5.00.

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AUSTRALIA

APMA (Australian Plastic Modeller's Association, PO Box 51, Strathfield, N.S.W. 2135)

1 83 (20 pages) "Re-Engine an Avia in 1:72" one page of text and drawings for converting KP's B.21 into a BH.33. "Passin' thru: Foreign Herks" 4 pages including 3 pages of drawings & details (Singapore '731' and Thailand '60103'). "Evolution of an Air Force: Rhodesia to Zimbabwe - Part 2" 2 pages including a page of drawings (Provost 'R3064', Hunter 'RRAF121', & Dakota.

AUSTRIA

OPH NACHRICHTEN (Oesterrichische Flugzug Historiker, Kannwegasse 1/15, A-1150, Wien)

2 83 (40 pages) "Mein Frontflug vom 6.12.1914" 12 pages including one page of drawings of Hansa Brandenburg B I '05.07'. "Feldpilot Ernst Kerschischnig" 4 pages including 2 pages of drawings of Hansa Brandenburg C I '429.29'. "Brunner Schwingenflugzeug" 4 pages including 1/72-scale drawings of ornithopter. "Osterreichische Piloten uber Spanien" 2 pages. "Schweden uber Osterreich" 9 pages including drawings of Skyraider 'SE-EBK' and Firefly 'SE-BRC'. Inserted into this issue was a 8 page booklet on the aircraft of Austrian Airlines which includes 33 color side-view drawings of the aircraft used by Austrian Airlines from the Junkers A20 of 1928 to Airbus A-310 221 of 1986. Among the less familar aircraft illustrated are the HS 829, Ph-M L-2/C, Ju G24, & Ju G31.

PANORAMA (IPMS/AUSTRIA, Nordmannngasse 11-13/4/6, A-1210 Wien)

Nothing received since 1/83.

BELGIUM

KIT (IPMS Belgium, Te Couwelaarlei 103 bus 21, B-2100 Deurne; 4 issues for 450 Belgian franks)

Nothing received since #48.

BRAZIL

EM ESCALA (IPMS-BRAZIL, Rua Arquias Cordeiro, 316 S/502, CEP 20770, Meier, RJ)

3/83 (26 pages) "Brazilian B-25" 4 pages including drawings of a/c in Brazilian markings. (The Brazilian Air Force received 6 B-25B in 1942. On 22 May 1942 one attacked the Italian submarine 'Barbarigo' which had attacked the merchant ship 'Comandante Lyra'. "Brazilian P-40E" 2 pages of drawings of Brazilian a/c in RAF desert camouflage.

CANADA

HIGH FLIGHT (Box 393, Stittsville, Ontario K0A 3G0; 6 issues for CAN \$15)

Nothing received since 11/12 82.

RANDOM THOUGHTS (IPMS CANADA, Box 626, Stn. B, Ottawa, Ontario K1P 5R7; 6 issues for \$12.00 in US, \$13.00 others)

Nothing received since 4 82.

ENGLAND

MAGAZINE (IPMS ENGLAND, Flat 4, 18 Delancey St., London NW1 7NH; 6 issues for US \$20.00 + \$1.00 joining fee)

2 83 (20 pages) "Buccaneer" 8 pages including 14 photos, 3 pages of drawings of RAF Buccaneers, and a pair of vacuformed slipper tanks. (The tanks are cleanly formed from very thick plastic and can be used on SAAF Buccaneer. A great idea that should be followed by other IPMS groups.)

FINLAND

MALLARI (IPMS FINLAND, PL 798, 00101 Helsinki 10; issues #41-44 US \$6.50, add \$1.00 for airmail and \$1.50 for personal bank cheque redemption)

Nothing received since #41.

FRANCE

LA VITRINE DU MAQUETTISTE (IPMS FRANCE, 3 rue de l'Amiral Roussin 75015 Paris; 4 issues 60FF surface, 80FF air; back issues 12FF; "La Lettre de l'IPMS" 25FF.)

1 83 (44 pages) "Le Heinkel He 46" 4 pages including a photo of Spanish He 46 '110157'. "Les SO 1220 et 1221 Djinn - 2^e partie" 6 pages with list of export Djinn and plans & templates for building a Djinn in 1/72 scale. "Le N.A. Rockwell OV-10A Bronco - 2^e partie" 8 pages including drawings of West German OV-10b 'D-9545' & OV-10b(z) 'D-ICON (with details of jet engine and rear crew position) and color schemes for Thai and Venezuelan aircraft.

GERMANY

MITTEILUNGEN (IPMS GERMANY, Oertzenweg 12b, 1000 Berlin 37; DM 42.00 Europe, DM 49.00 USA, DM 55.00 Japan, DM 61.00 Australia)

6 83 (30 pages) "Northrop F-5E als Maschine der Luftwaffe von Kenia" 2+ pages including 3 photos of model and one page of drawings.

7 83 (30 pages) Nothing of small air force interest.

ITALY

IL NOTIZIARIO (IPMS ITALY, CP 182, 41100 Modena Ferrovio)

1 83 (32 pages) "Dewoitine a l'Italienne" 2 pages including drawing and 3 photos of cockpit interior. "Aardvarks from Down Under" 4 pages including 8 photos. "Bleriot XI" 9 pages including 10 photos, 1/72-scale drawings for the "Aircraft used by Capt. Carlo Piazza in the first war mission (a recce flight) of aviation history. Tripoli, Libya, October 23, 1911.", large-scale drawings for the S.I.T-Bleriot 80 HP 1914" (two seater), and drawings for converting the Frog/Novo kit to Piazza's aircraft.

JP-4 (CP 1550, 50100 Firenze; L 27.000 Europe, L 30.000 elsewhere)

4 83 (88 pages) Photo: Sudan AB.212 (color). "La Difesa Aerea Metropolitana durante la Grande Guerra" 5 pages including 9 photos of Voisins.

5 83 (116 pages) Photos: Venezulean RF-5A '8707' with new national insignia (color) and Spanish SAR Super Puma (color). "Speciale Elicotteri" 5 pages including color drawings of German Bo.105, Italian AB.206A, and Dutch Alouette III "Grasshopper"; and list of all helicopter kits in 1/32 and 1/48 (and similar) scales.

NEW ZEALAND

SCALE DIMENSIONS (IPMS-NEW ZEALAND, 43 Jilliteresa Crescent, Bucklands Beach, Auckland; US\$ 10.00 surface or US\$ 15.00 airmail)

May 83 (28 pages) "Kiwis with the RAF: De Havilland Mosquito with No. 487 (New Zealand) Squadron" 3 pages with one page of drawings of Mosquito FBVI 'EGOT'. "Aircraft of the RNZAF No. 7: Bell UH-1" 14 pages including 4 photos, 3 pages of drawings of details, 5 pages of scale drawings showing all 6 different color schemes carried by RNZAF Iroquois (including all gloss white aircraft of the Multinational Force and Observers in the Sinai Desert), and colour chips of most recent colors.

NORWAY

LINTUBEN (IPMS-NORWAY, PO Box 273, 2050 Jessheim; US\$ 9.00 in Europe and US\$ 11.00 in USA/Canada airmail)

Nothing received since 2/80

SINGAPORE

MODELLERS' MONTHLY (Plastic Modellers Society Singapore, 247 Macpherson Road, Singapore 1334)

Nothing received since 1-2 83.

(Continued on page 31)

RWD Aircraft around the World

(Editor's note: This is a translation of the article "Samoloty RWD na szerokim swiecie" by Andrzej Glass. It was first published in *TECHNIKA LOTNICZA I ASTRONAUTYCZNA* #5/82, and was translated by Kris Choloniewski (SAFCH #96). Permission to publish the translation in the *SAFO* was graciously given by Mr. Glass, Editor in Chief of *TECHNIKA LOTNICZA I ASTRONAUTYCZNA*.)

In the 1930s, there were only eight nations in the whole world that were exporters of aircraft. Poland was one of these, exporting the products of the PZL, RWD, and PWS factories.

The first RWD aircraft sold abroad were two RWD-9 sport airplanes which were obtained by Czechoslovakia to represent this nation in the 1934 Challenge de Tourisme International. These aircraft were registered OK-AMC and OK-AMD. The same year France purchased one RWD-9 (F-AKHE) for experimental purpose and two RWD-9s were sold to Spain the next year.

In 1934 a license to manufacture the RWD-8 training aircraft was granted to Estonia, and one airframe of this type (ES-RWD) was completed before the end of the year. Two aircraft of this same type were purchased by Palestine where they operated as VQ-PAG and VQ-PAK. The Rogozarski plant in Yugoslavia acquired a license to build the RWD-8 powered by the Czechoslovakian Walter NZ engine, but probably only a few of these aircraft were complete before the German invasion of Yugoslavia. It is known that two aircraft of this type (YU-PCY and YU-PDM) were operated by the aeroclubs of Belgrade and Novi Sad respectively.

Export of the RWD-13 touring aircraft began in 1936. A license and four aircraft of this type were purchased by Spain. No use was made of the license because of the Civil War, but the RWD-13's were impressed into the Nationalist Air Force where they operated as liaison aircraft. They carried the codes 30-1 to 30-4 and were nicknamed "Polaca". In 1941 two of them were in use with the aeroclub at Saragossa, and in 1948 they were with the aeroclub at Legrono. Six RWD-13's were sold to Brazil in 1937 where they remained in service for many years, and two were sold to Venezuela. One RWD-13 was purchased by Austria and one by Estonia with the latter serving on the Tallin - Riga route. Two were sold in the USA where they were used as air taxis under the registrations N20651 and N20652; one of these was used until 1950. With Greek assistance, three RWD-13's were sold to Yemen. The Palestinian air company Aviron had three RWD-13's in use; one acquired directly from Poland, and two purchased from Yemen. These were registered VQ-PAF, VQ-PAL, and VQ-PAM. One of them was among the aircraft used to establish the Israeli Air Force. In total, at least 20 RWD-13's were exported. In addition, several aircraft of this type were built under license by Rogozarski in Yugoslavia where they were used both by aeroclubs and the Yugoslav Air Force. In the spring of 1939, one RWD-13 (SP-BNY) was handed over to Iran as a wedding present for Mohammed Reza, successor to the throne.

About this same time, one RWD-15 touring/executive aircraft was exported to Palestine where it received the registration VQ-PAE. After the War, this aircraft was used by the Aviron company.

The German invasion of Poland resulted in the evacuation of about 160 RWD aircraft to other countries where they were impressed into military service. On 17 September 1939, 61 RWD-8's crossed the border into Rumania (48 civilian and 13 military), about 40 escaped to Latvia, several to Hungary, and two to Finland. Several RWD-8's left on the airfield at Biala Podlaska were taken over by

the Soviet Air Force, and several more were obtained when the Soviets invaded Latvia. The RWD-8 was used extensively by Rumanian civil and military units. Two RWD-8's served with the Hungarian Air Force under the codes I-281 and I-282, and subsequently by civil authorities as HA-RAA and HA-RAB. One RWD-8 was repaired at Mielec and tested by the Germans. A photo shows this aircraft in standard Luftwaffe markings without the usual swastika on the vertical fin.

On 14 September 1939, one RWD-13 (SP-BML), with pilot E. Jereczek at the controls, escaped from Gdynia-Okęcie to Viabergslatt, Sweden. During the war this aircraft was used by the Swedish Air Force under the type designation TP-11 and after the war as a civil aircraft bearing the registration SE-AOF until 1951.

In mid September 1939, 25 RWD-13 aircraft were evacuated to Rumania. During the war these aircraft were impressed into the Rumanian Air Force for liaison and ambulance service, subsequently being handed over to civil aviation. Four of these aircraft returned to Poland after the war, receiving the registrations SP-MSZ, SP-ARG, SP-ARH, and SP-ARL. SP-ARL now takes a well-earned rest in the Muzeum Lotnictwa i Astronautyki in Krakow.

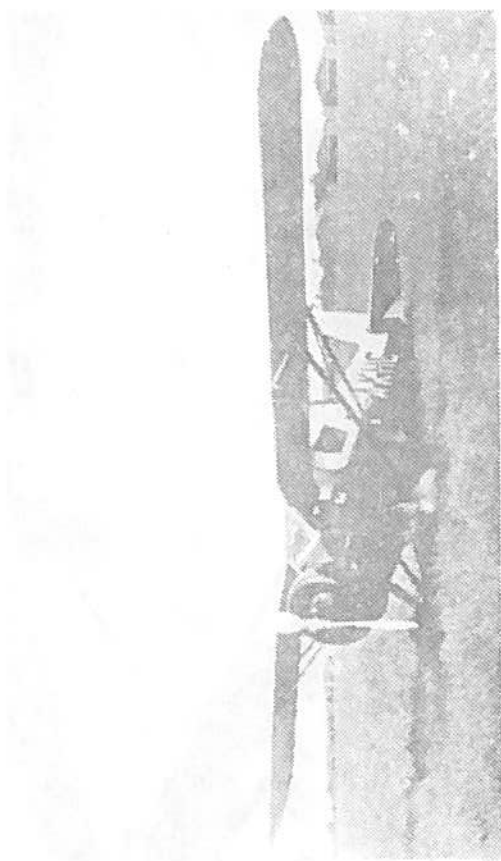
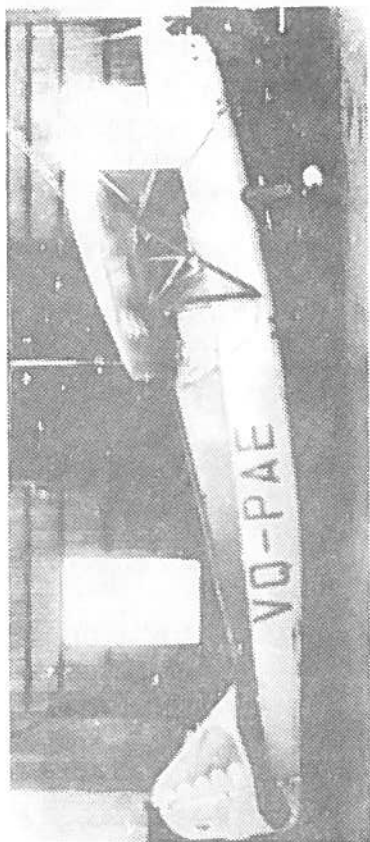
In September 1939, about ten RWD-14 Czapla (Heron) observation aircraft were evacuated to Rumania and one escaped to Hungary. Two RWD-15 (SP-ALA and SP-KAT) landed in Rumania where they were given the registrations YR-FAN and YR-TIT. Also evacuated to Rumania were 7 RWD-17 aerobatic trainers (SP-BMX, SP-BOJ, SP-BOU, SP-BOW, SP-BOY, SP-BOX, and SP-BOZ); 3 military RWD-17W (on land undercarriages); and 2 RWD-21 light touring aircraft (SP-BPE and SP-BRH). SP-BPE served in Rumania as YR-VEN and returned to Poland after the war to be used until 1950 under the registration SP-AGK, after which it was retired to the Muzeum Lotnictwa i Astronautyki. One RWD-21 was evacuated to Latvia.

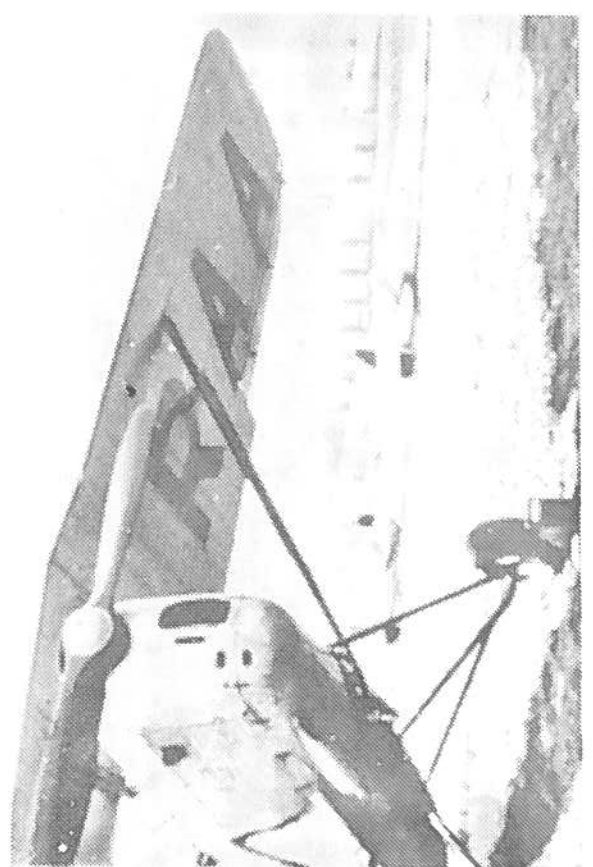
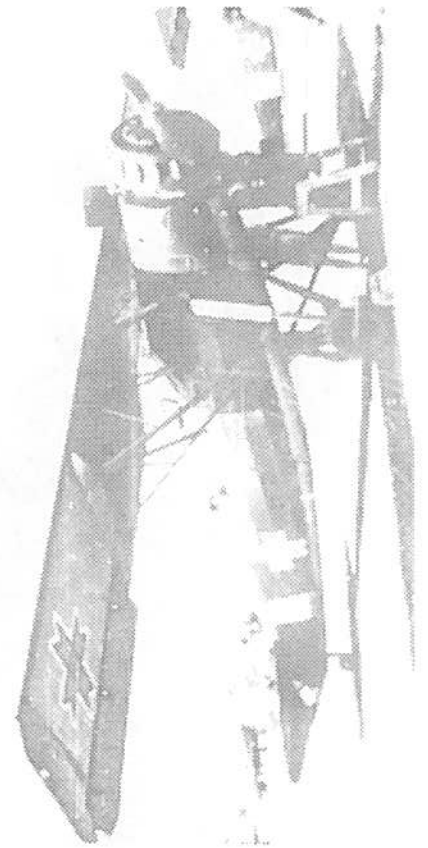
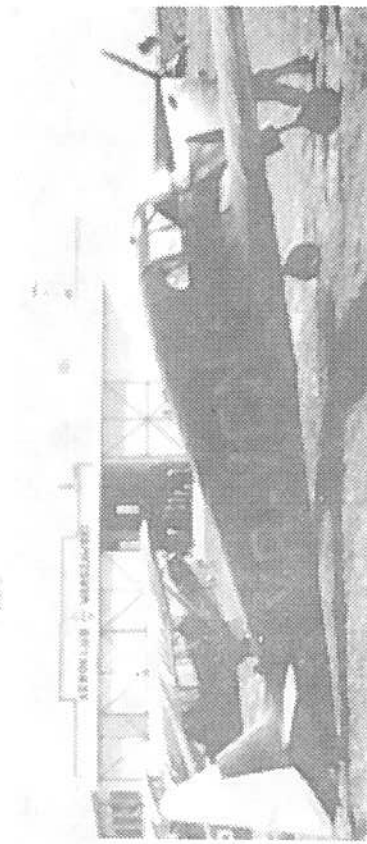
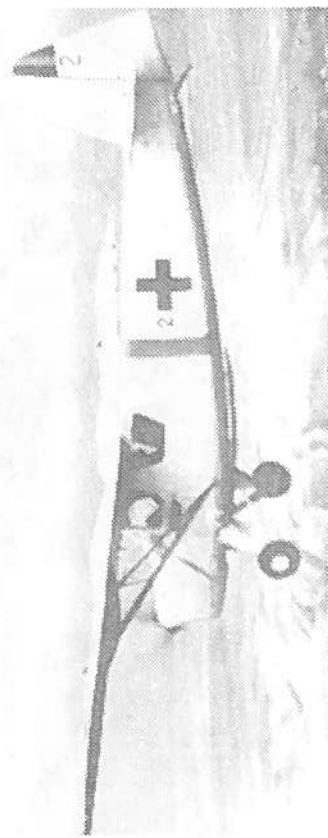
Of the 111 RWD aircraft evacuated to Rumania, at least 50 were still in flying condition at the end of the war. These aircraft were identified in list prepared by Polish aviation authorities and handed over to the Rumanian Government. However, the Rumanian authorities took no action and only 5 of these aircraft were returned to Poland, mainly because of the individual efforts of Polish pilots, especially K. Kasprzyk.

Of 850 RWD aircraft built in Poland before the war, about 190 found their way out of Poland. When consideration is given to the RWD aircraft built under license, over 200 RWD aircraft were operated in 19 countries other than Poland.

(Editor's note: The following is a translation of a short article which originally appeared in *TECHNIKA LOTNICZA I ASTRONAUTYCZNA* #7/82 to accompany a very unusual photograph. The article was titled "Jeszcze o samolotach RWD na szerokim swiecie" and permission to reprint this translation is as above.)

We are proud to publish the heretofore unknown photo of a RWD-14b Czapla in Rumanian markings. This photo was provided by Malcolm Passingham of England. It is seen that the colors of this aircraft were similar to that used in Poland, i.e., dark olive upper surfaces and light blue under surfaces. The Rumanian national insignia is carried in six positions in addition to the usual tri-color vertical stripes on the rudder, the code number on the fin, and a yellow band around the rear of the fuselage. According to Rumanian sources, 11 aircraft of this type were impressed into the Rumanian Air Force.





RWD Aircraft Evacuated to Rumania

RWD-8 dwl				40	BHF	CAB	34-360	19	BNU	6	283
1	SP-ALB	YR-ANA	64	41	BHG	-	34-361	20	BNW	-	284
2	ALO	AMD	66	42	BHH	CFK	34-362				
3	ANM	ANB	94	43	BHI	BRM	34-363	RWD-13S			
4	AYG*	BRG	123	44	BHK	CAC	34-364	21	BJM	-	-
5	AYK	AOB	126	45	BHP*	PRI	34-368	22	BYM	2***,INT	261
6	BCD	BCD	138	46	BJA*	PRX	34-378	23	69K	-	262
7	BKH	PRH	188	47	BJE*	CFL	34-382	24	BJU	-	278
8	BKN	BRT	193	48	BJF	CFI	34-383	25	-	-	307
9	BKP	CFG	195	49	BJH*	AOD	34-385				
10	BLA	AND	204	50	BJL*	CRF	34-388	RWD-14 Czapla			
11	BLB	ANC	205	51	BJS	PSA	34-394	10 Polish Air Force aircraft			
12	BLF	ANE	209	52	BJT	CAD	34-395				
				53	BJY	BRB	34-399	RWD-15			
RWD-8 pws				54	BJZ	-	34-400	1	SP-ALA	YR-FAN	337
13	**	-	34-7	55	**	CFM	34-414	2	KAT	TIT	335
14	**	-	34-44	56	**	PRY	34-416				
15	**	YR-AOH	34-45	57	**	AOG	34-417	RWD-17			
16	SP-APB	PAC	34-62	58	**	CRD	34-453	1	SP-BMX	YR-AMH	254
17	APG	AOA	34-67	59	BGS	AOF	34-461	2	BOJ	CAE	277
18	**	BRK	34-134	60	BSK	AMB	34-464	3	BOW	-	289
19	AYW	AOC	34-156	61	BSN	-	34-467	4	BOU	BOU	297
20	**	PRZ	34-170					5	BOY	AME	298
21	AZP	-	34-186	RWD-13				6	BOZ	CAG	299
22	BAM	BRA	34-206	1	SP-ATA	YR-EUG	113	7	BOX	CAF	301
23	BAW	AOE	34-214	2	ATH	-	131	8	BRA	-	-
24	BAZ	BRH	34-217	3	ATK*	-	133				
25	BBL	BRI	34-228	4	ATJ	PSD	148	RWD-17W			
26	BDE	BRD	34-236	5	BFG*	-	160	9	**	CBA	311
27	BDK	AMC	34-241	6	WDL*	-	169	10	**	BRR	312
28	BDP	-	34-246	7	- *	ISD	170	11	**	AMI	314
29	BEG	CFI	34-261	8	BFS	PSI	173				
30	BEO	PAA	34-268	9	BFT	ROV	174	RWD-21			
31	BES	CFH	34-271	10	BFU*	5***	175	1	SP-BPE	YR-VEN	-
32	BET	PRS	34-272	11	BFW	-	213	2	BRH	-	331
33	**	BRF	34-282	12	BMJ	12***	214				
34	**	AME	34-287	13	BMK	BMK	215	-	Registration not known.		
35	**	BRC	34-332	14	BMM	-	217	*	Registration not applied.		
36	**	CRA	34-353	15	BMO	-	219	**	Polish Air Force registration		
37	BHA	BRE	34-355	16	HZM	-	221		same as c/n.		
38	BHB	BRL	34-356	17	BMT	9***,BMT	224	***	Rumanian Air Force		
39	BHC	CAA	34-357	18	BME	AMF	226		registration.		

RWD-8 in Hungary

(Editor's note: An article written by Laszlo Winkler and entitled "Przyloty polskich samolotow na Wegry we wrzesniu 1939 r." appeared in TECHNIKA LOTNICTWA I ASTRONAUTY-CZNA #1/83. It discusses the Polish aircraft evacuated to Hungary in September 1939. Among the aircraft interned were two RWD-8's, and the relevant parts of this article are presented here to provide further coverage of RWD aircraft which served outside of Poland. This translation is reprinted with permission of Mr. Glass.)

On 18 September 1939, the Commanding Officer of Ungvar Airfield in Hungary reported that a RWD-8 with two Polish officer pilots had landed at Munkacs. Hungarian engineers were sent to Munkacs, the aircraft was repaired, and both the plane and its crew were brought to Ungvar Airfield. (The aircraft carried the number '152' on its fuselage and the center section of the wing was painted white indicating that it came from the Training Center at Deblin).

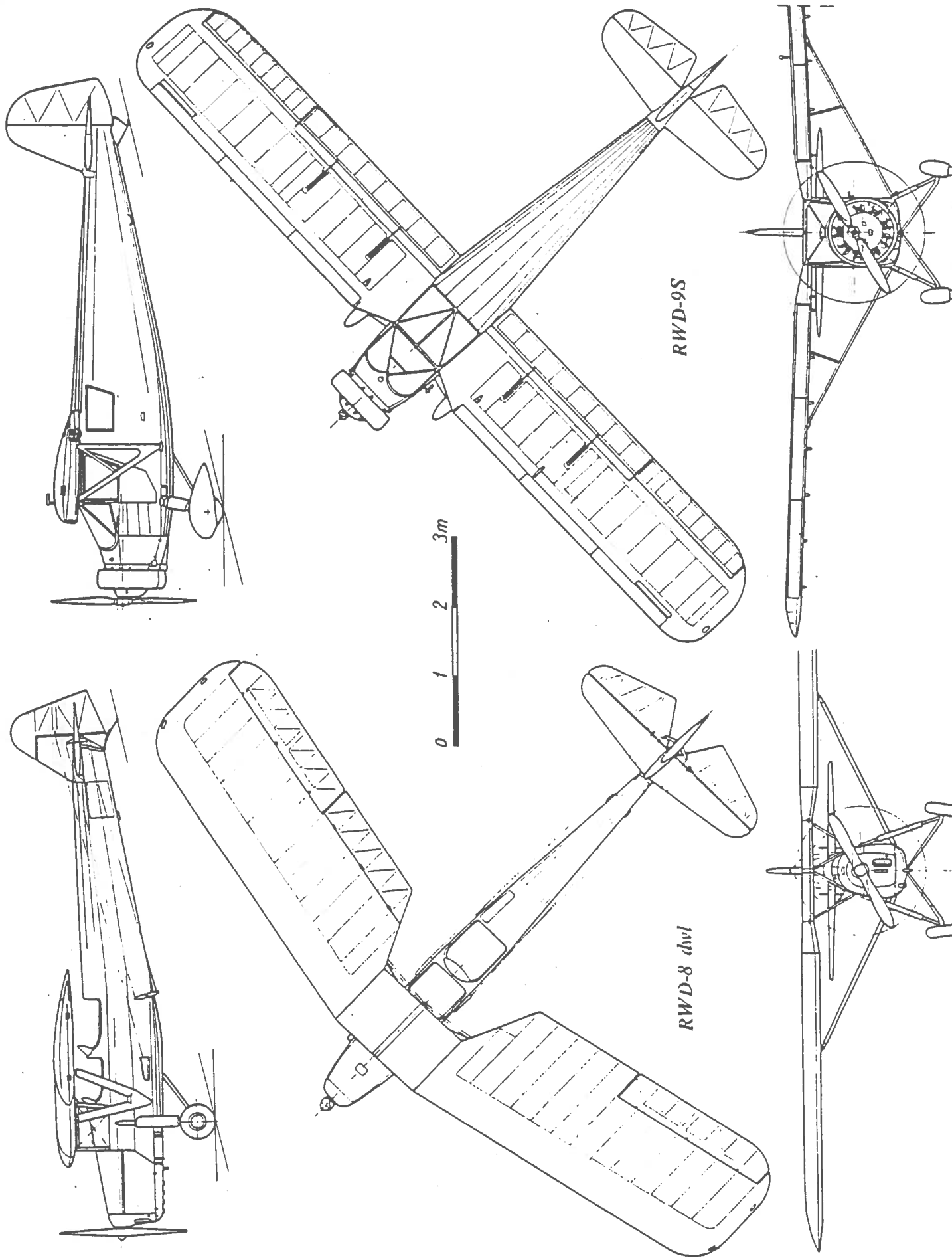
On 25 September 1939, the Command Post of Hungarian Gendarmerie at Aknaslatina issued the following report: "A Polish airplane piloted by a corporal pilot landed west of Aknaslatina near the cemetery of the village of Tarackoz. The plane has been secured by Cpt. Pozsgay, the Commanding Officer of the railway station. Suitable reports to the Mountain Rifle Battalion at Beregszasz have been prepared." A Hungarian Military Aviation Commission

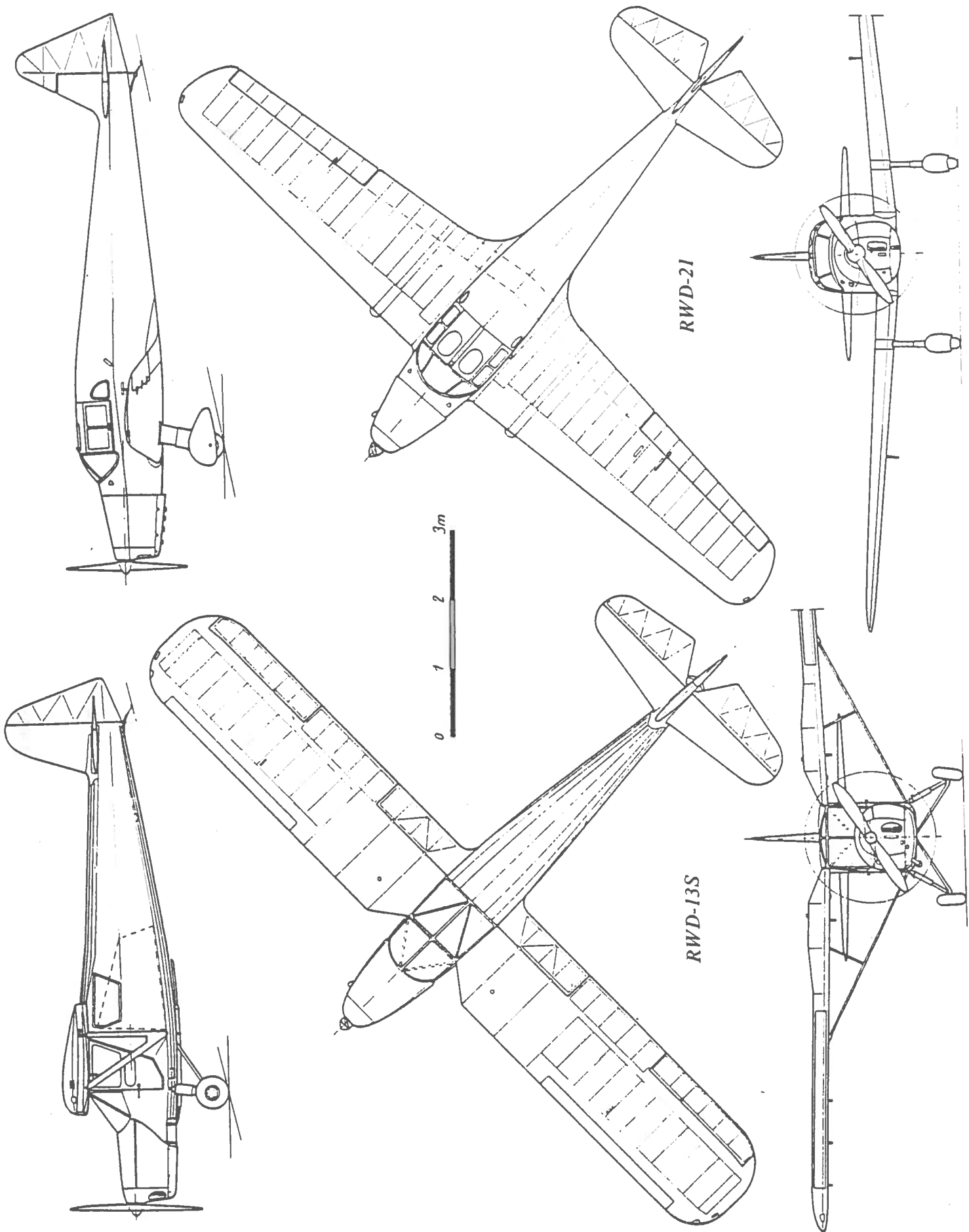
sent to the site identified the aircraft as an RWD-8.

These RWD-8 trainers were given military registrations 'I-281' and 'I-282'. These aircraft retained their original color schemes of kakhi and light blue. The registration was applied to the fuselage and the Polish national insignia were replaced by the Hungarian tricolored chevrons.

In the summer of 1940, the two RWD-8's were withdrawn from Hungarian military service and they were handed over to the civilian National Aviation Fund in the name of Miklos Horthy. These aircraft, together with Hungarian Gerle aircraft, were used as glider tugs. Their reliable engines and excellent climbing performance were greatly admired by the Hungarian pilots. One of these RWD-8's was repainted overall silver and received the civilian registration 'HA-RAA'. The other was registered 'HA-RAB', but it retained its previous color scheme. Their different colors made in-flight-identification of these planes very easy. These aircraft participated in all the sport-aviation events and flying-model contests held at the famous Rakosmozo Airfield where they were used by the judges among others.

On 1 July 1942, all pilot-training centers in Hungary were taken over by the central organization MAVERO. The two RWD-8 trainers served with this organization until nearly the end of WWII at which time they were destroyed.





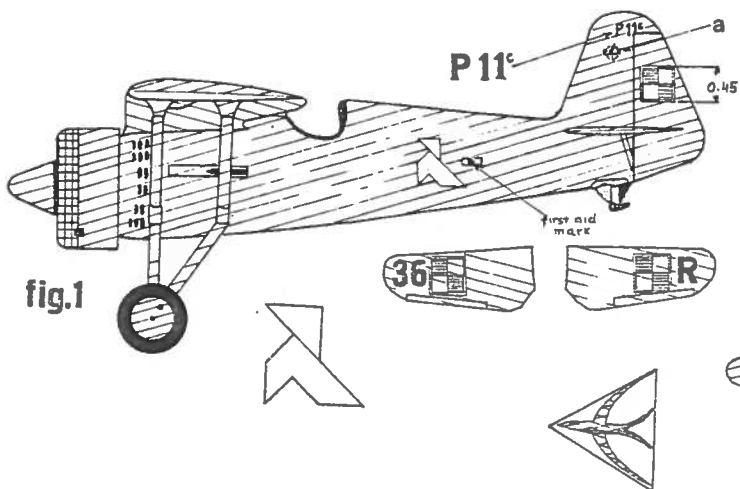


fig.1

PZL P 11c

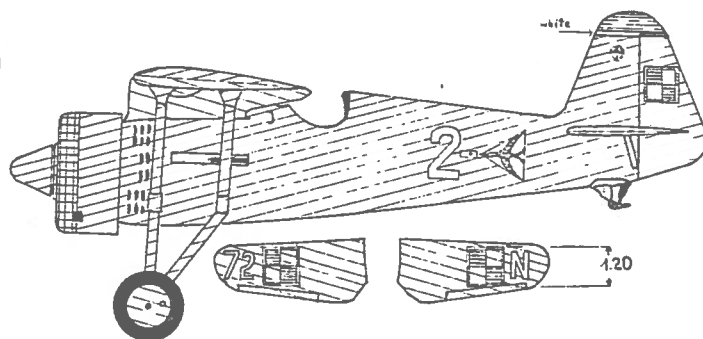


fig.2

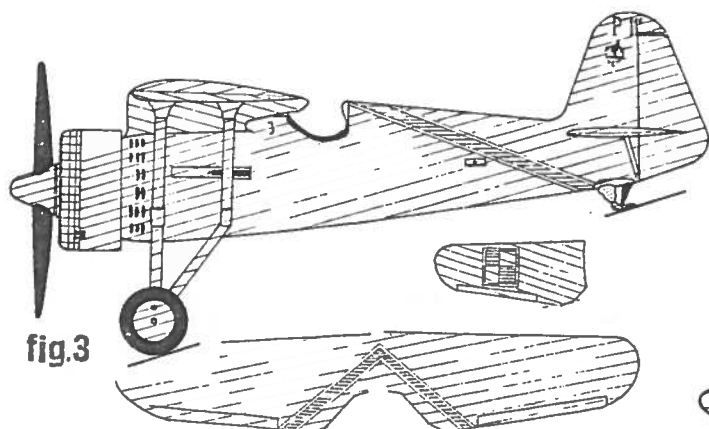


fig.3

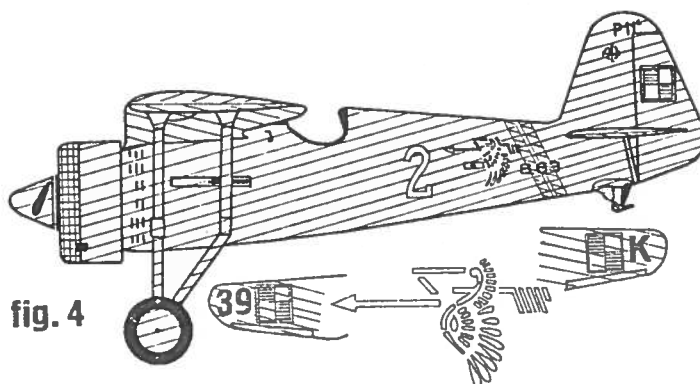


fig.4

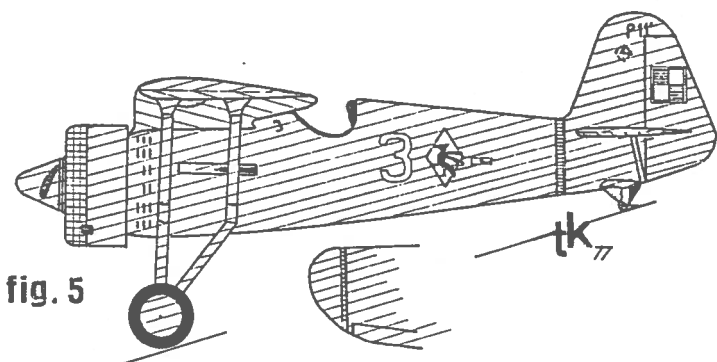


fig.5

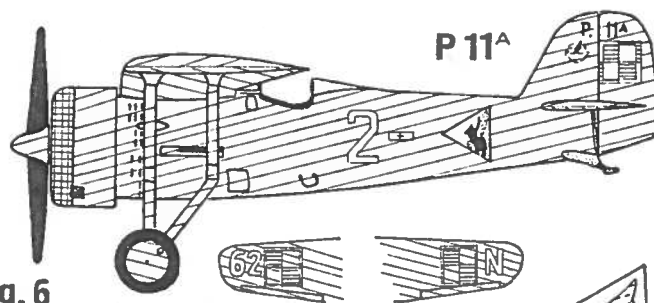


fig.6



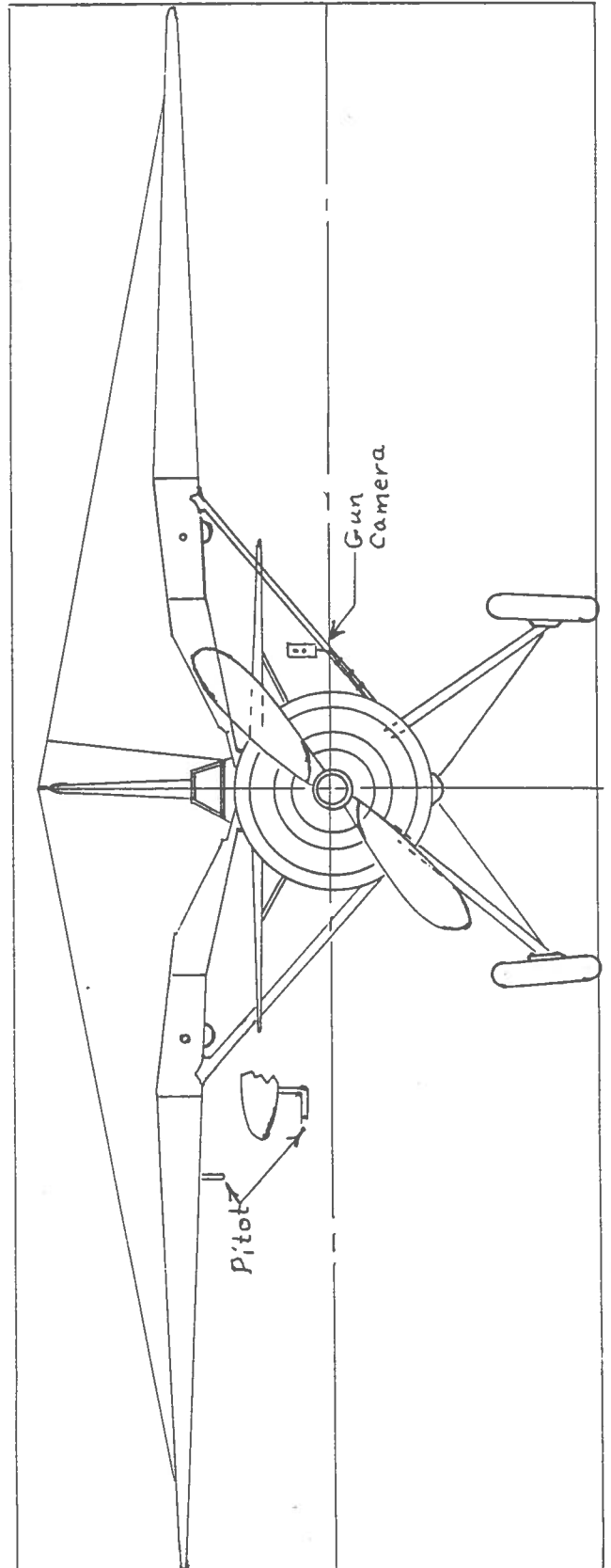
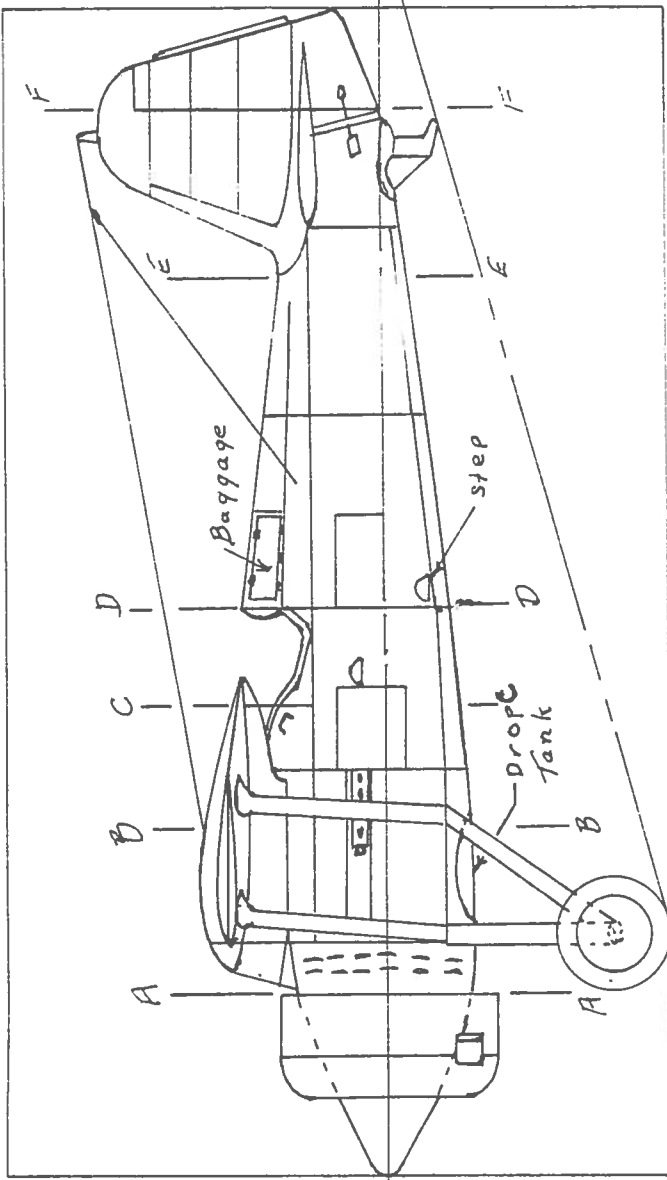
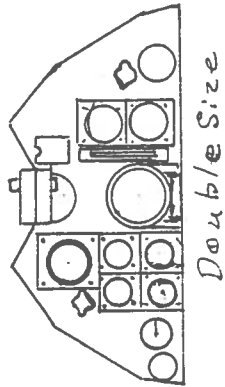
1. P-11c of CWL at Deblin. A/C received from 122 Sq. of 2nd Air Regiment at Krakow.
2. P-11c of 114 Sq., 1st Air Regiment.
3. P-11c - personal a/c of General Ludemił Rayski, CO of Polish Air Force.
4. P-11c of 121 Sq., 2nd Air Regiment. Pilot - Wacław Krol.
5. P-11c of 131 Sq., 3rd Air Regiment. Pilot - Gedymin CO of flight.

6. P-11a of 113 Sq., 1st Air Regiment.

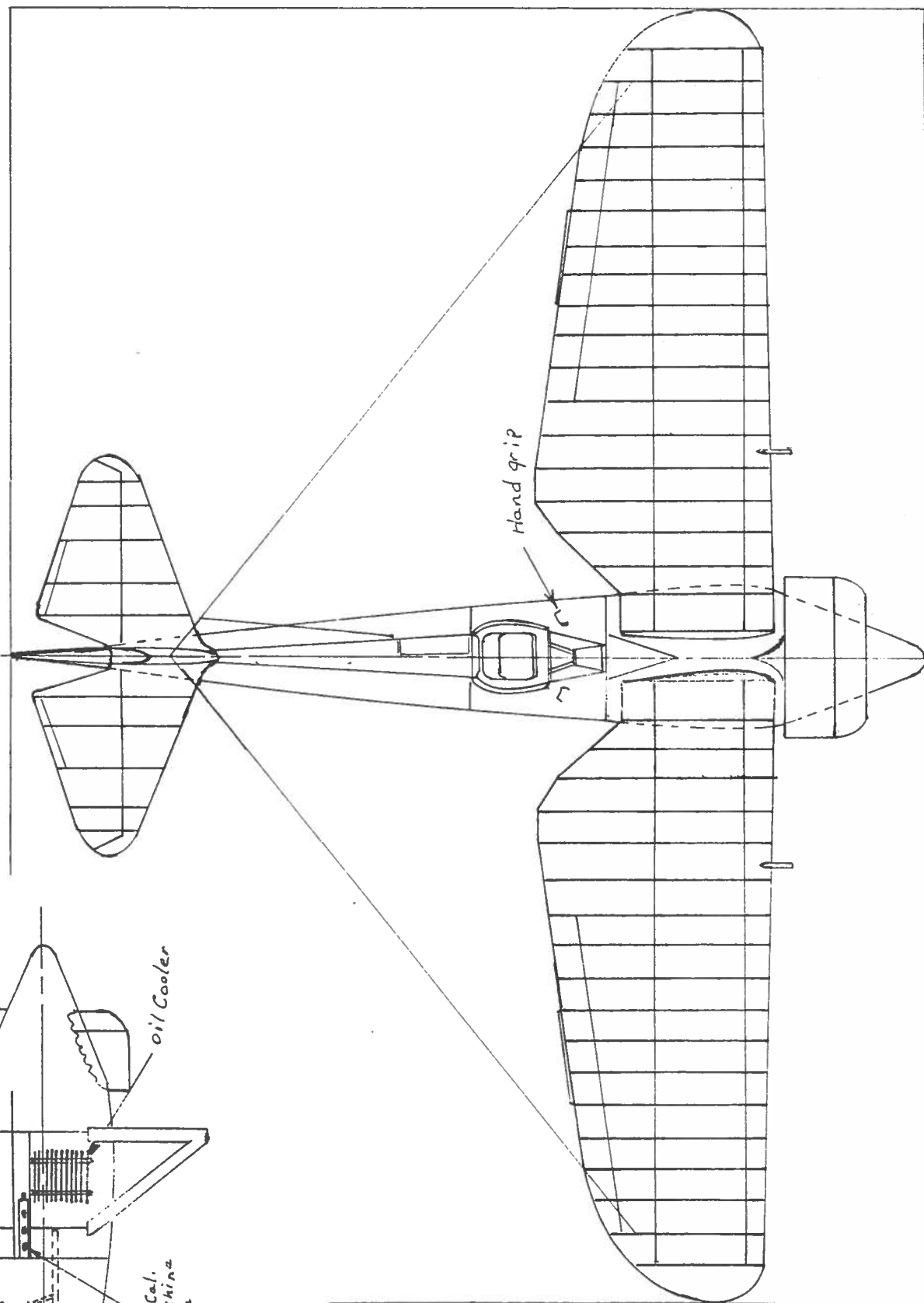
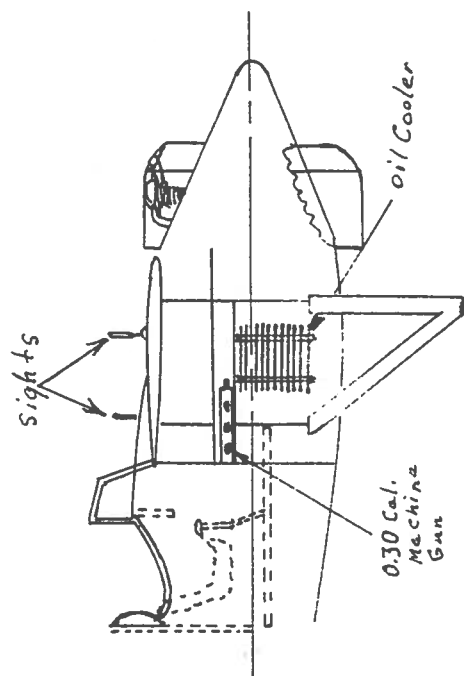
Color Key: a. Yellow. b. Red. c. Rust. d. Black. f. Dark Blue. g. Olive Green. h. Brown. k. White.

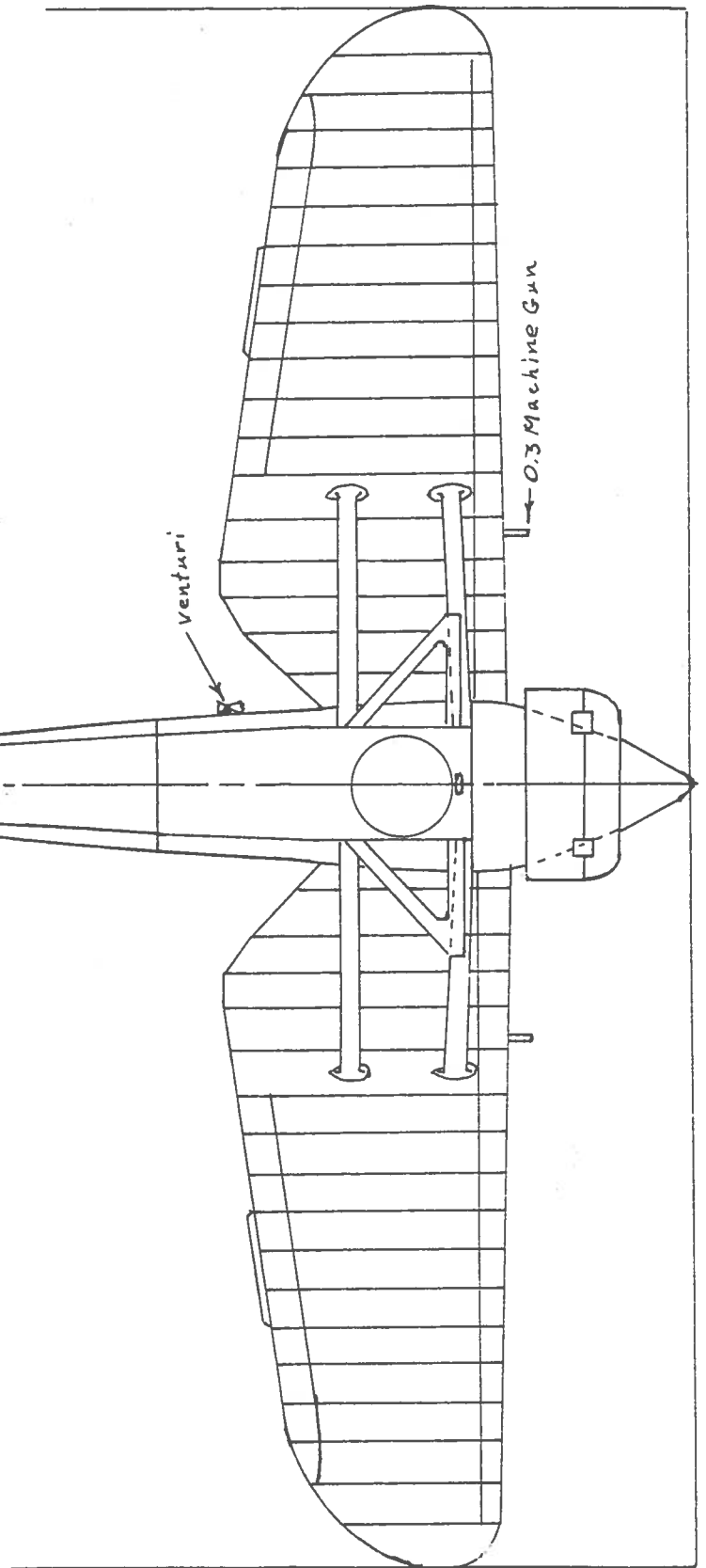
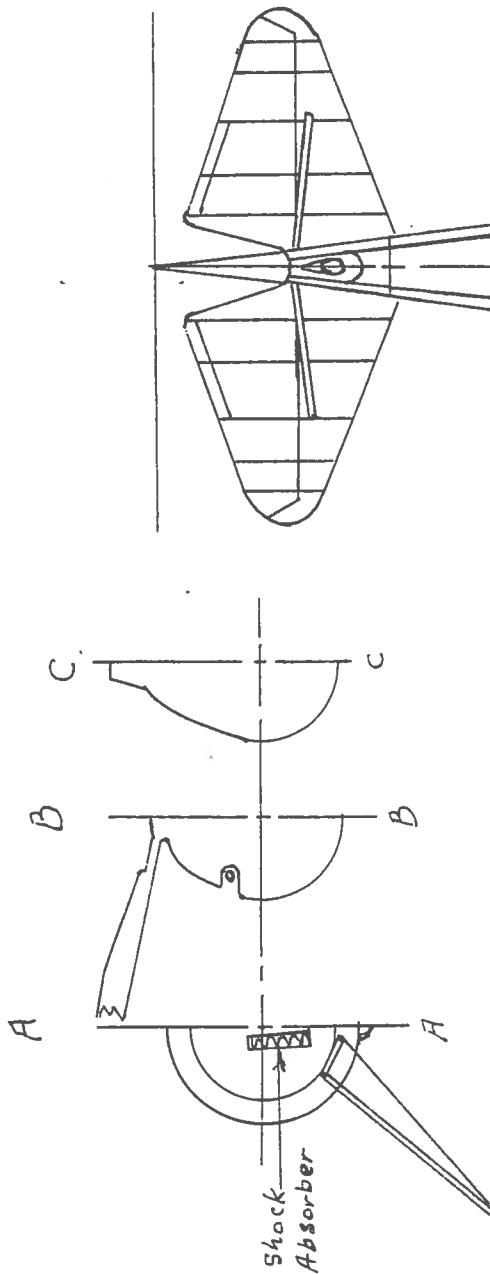
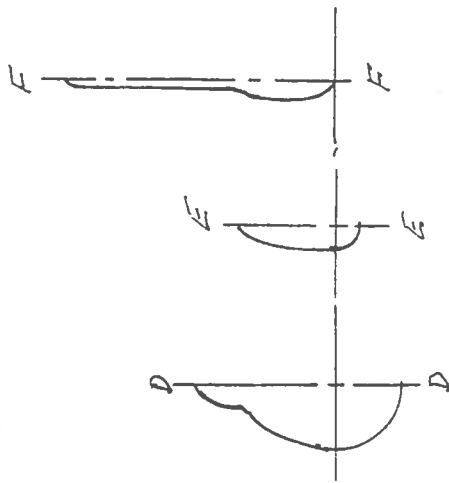
Tomasz Kowalski (SAFCH #121), ul. Moniuszki 30m3, 87-100 Torun, POLAND

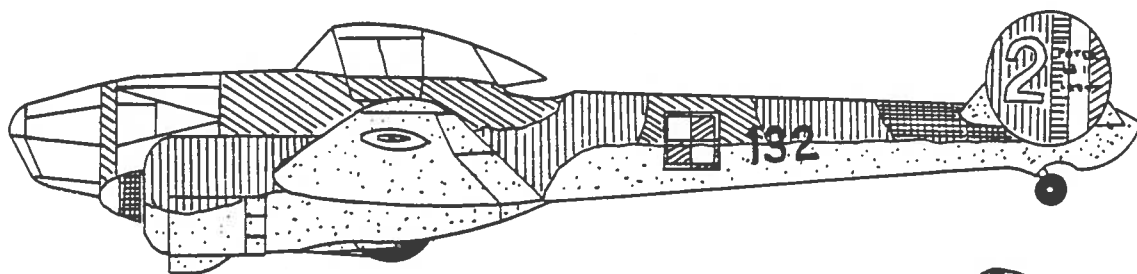
Aircraft of the Small Air Forces in 1/48 Scale



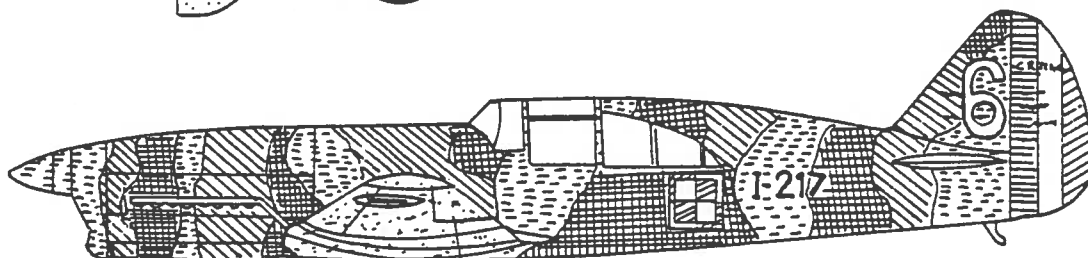
Scale = 1:48	PZL - P11C
Drawn By Wayne A Denny	Państwowe Zakłady Lotnicze (Central Aviation Workshops)



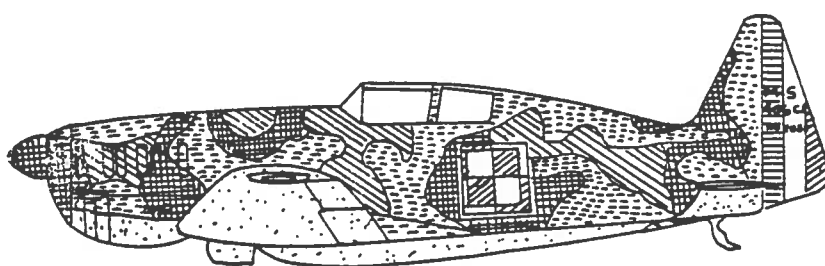




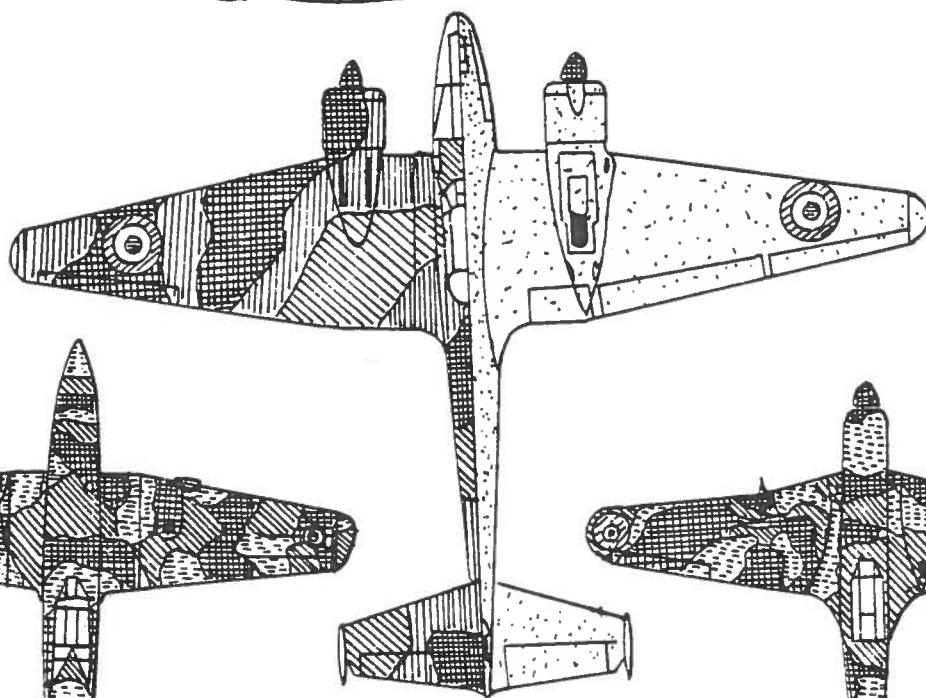
A



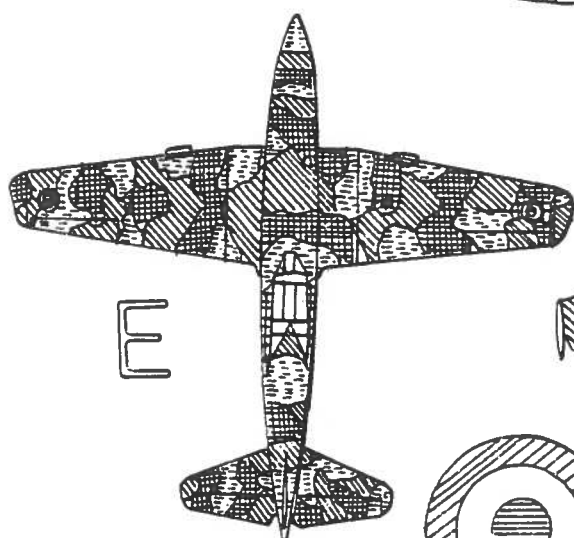
B



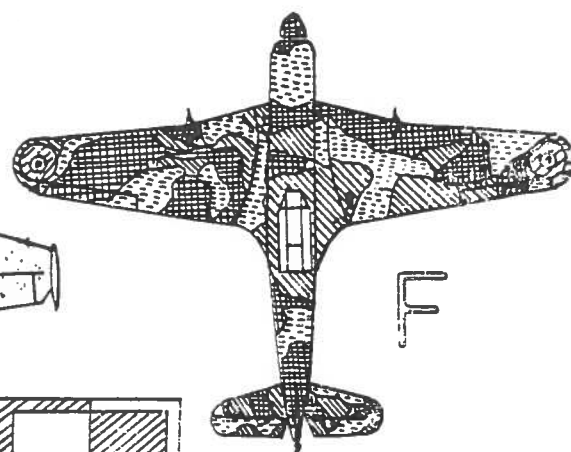
C



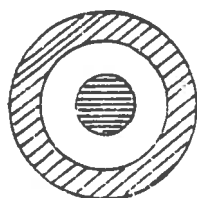
D



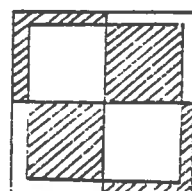
E



F



G



H



Polish Air Force in France

With the invasion of Poland in September 1939, a hard fought battle followed which resulted in the complete destruction of the Polish Air Force. The modern equipment, and sheer weight of number, employed by the Luftwaffe ensured the virtual annihilation of the modest Polish air arm. However, a great number of Polish Air Force personnel managed to escape from the country and after many hardships they made their way to France where they volunteered to continue the fight.

Several all-Polish units were formed within the French Air Force and some became operational in time to participate in the Battle of France. These units were equipped with French machines marked with the Polish Air Force insignia. The following examples show three of these machines:

Fig. A. Potez 63.11 of the Observation Instructional Escadrille at Lyons-Bron, April 1940. This machine is finished in chocolate brown (98)*, stone (71), and dark green (76), with the undersides in pale blue (hg 5). The tip of the spinner is yellow. The number "2" is in white and the aircraft name "Potez 63.11" on the tail and the number "132" on the side of the fuselage are black. The finishes of the upper and lower surfaces are shown in Fig. D. French roundels appear on both surfaces of the

wings and the tail carries standard French tail markings.

Fig. B. Caudron-Renault CR 714 in use during June 1940 near Rouen. It is finished in chocolate brown (98), dark green (76), and pale grey (64), with the undersides in pale blue (hg 5). Fig. E shows the finish of the upper surfaces. The number "6" is in white and the inscriptions "I-217" on the fuselage and "CR 714" on the tail are in black. The national markings are as for Fig. A.

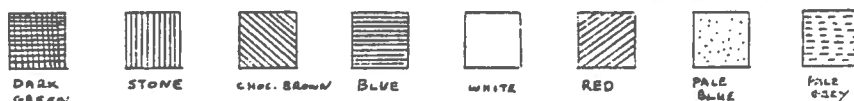
Fig. C. Morane-Saulnier MS 406 attached to Groupe de Chasse 111/2, June 1940. The colour scheme is as for Fig. B, but with the inscription "MS 406 No. 1031" on the tail in black. Fig. F shows the upper surfaces of this machine.

Fig. G & H. Two types of national markings.

All of the above aircraft are available in 1/72 scale: Potez 63.11 Heller, CR 714 Heller, and MS 406 Heller & Frog/Novo.

*The numbers after the colours are Humbrol designations.

Bob Massey (SAFCH #364), 108 Worrall Ave., Arnold, Nottingham, ENGLAND



Cover Comments

COVER COMMENTS: The pair of RWD-8's in Hungarian markings on the cover serves to introduce a very special article by the well-known Polish aviation historian Andrzej Glass. Mr. Glass has given the SAFO permission to publish a translation of his text, along with some of his photos and drawings. He has done this in the hope that SAFO readers will be able to supply additional information on RDW aircraft operated outside of Poland. If any of you have such information, please send it to the SAFO editorial office and I will publish it in the SAFO and pass it on to Mr. Glass.

Of interest are the markings, registrations, serial numbers, manufacturers numbers, fates, and photos of any RWD aircraft outside Poland: e.g., RDW aircraft in Austria, Lithuania, Latvia, Estonia, Palestine, USA, Brazil, Venezuela, Spain, Iran, & Yemen. Copies of the scale drawings on page 7 and 8 can be used to provide marking information. Mr. Glass particularly asked for help with the following question: How many RWD-8 and -13 were built in Yugoslavia and were any of these fitted with a radial engine?

Any reader in the USA with access to the FAA files should be able to provide interesting information on the RWD-13's in US registration, and perhaps similar opportunities exist for readers residing in the other countries listed above. A good response to Mr. Glass' request would be not only be an excellent demonstration of the SAFCH's unique value in promoting international cooperation in aviation research, but it would be a nice way to thank Mr. Glass for his kindness. In addition, a good response would encourage other editors to provide similar articles for translation and reprinting in the SAFO.

CAPTIONS FOR PHOTOGRAPHS

Cover

Top: RWD-8 in Hungarian Air Force markings, 1940.
Bottom: The olive-green RWD-8 (HA-RAB) in Hungary, 1941.

Page 5

F-AKHE: RWD-9 sold to France in 1935.
ES-RWD: RWD-8 built in Estonia under license.
Captured RWD-8 in Luftwaffe markings in early 1940.
RWD-8 (VQ-PAG) in Palestine after WW II.
RWD-13 (VQ-PAE) in Palestine.
RWD-13 (ex SP-BML) served with the Swedish Air Force with the designation Tp-11.

Page 6

RWD-13 '2' in Rumanian service.
RWD-8 (YR-BRI) in Rumanian Air Force markings.
RWD-8 (MA-RAA) in Hungarian service with its overall silver paint finish.
RWD-13 (YR-BMT) at Lodz, Poland, on its return to Poland after WW II.
RWD-21 (YR-VEN) at Warsaw on its return from Rumania after WW II. Note the Polish Piper Cub (SP-AMF) in the background.
RWD-14 Czapla '37' in the markings of the Rumania Air Force.

All photographs via author.

CANADIAN CF-18 MARKINGS, 1/72 scale, WMB Graphics, Box 13, Islington Stn B, Etobicoke, Ontario, CANADA M9C 4X9. CAN\$ 2.99.

With the acceptance of the first CF-18s by the Canadian Armed Forces/Forces Armees, the modeler of aircraft of the small air forces finally has a reason to purchase one of the many F-18 kits that have been recently released. This urge should be further promoted by the fact that the Canadians have released decals for the CF-18 in all three popular scales. (See letter in this issue from Michel Barnes.)

The sheet from WMB Graphics is for 1/72 scale. Measuring 16 cm by 6 cm and printed in blue and grey, this sheet contains all the national insignia, inscriptions, serial numbers, jet-intake warnings, rescue arrows, and ejection-seat triangles needed to finish one single seater and one two seater, with enough serial numbers included so that you can finish your models as any CF-18 that catches your fancy.

The instruction sheet provides top-, bottom-, and side-view drawings of sufficient detail that no further references are really necessary. Airframe colors are called out as blue (FS 35237) topsides and gray (FS 36375) undersides with Testors #1721 and Humbrol HU19 given as the match for the former and #1728 and USN1 for the latter. Also specified are the colors for the cockpit interior and for the antennae, wheel wells, etc. The Hasegawa and Italeri kits are recommended and all modification necessary to produce the Canadian version of the Hornet are described.

While I have not seen any photos of the CF-18 in squadron service, these may be applied by the time you read these words. Quite wisely, WMB Graphics has included a selection of squadron markings that should be applicable when the time comes.

References on the CF-18 have been fairly common: HIGH FLIGHT Nov/Dec '82 has an article on the delivery of the first aircraft to the CAF which includes 17 photos. (Could anyone resist the temptation to build a diorama of Prime Minister Trudeau climbing out of a CF-18 after a demonstration flight?) A excellent color photo of the first CF-18 appears in the June '83 issue of AIR INTERNATIONAL.

While you are picking up a Hornet kit for your CF-18, you might as well pick up two more. The Australian and Spanish aircraft will be coming along in the near future, although I doubt that we'll be lucky enough to have customized decals for these versions.

Review copy supplied by Michel Barnes (SAFCH #502).

242 SQUADRON, THE CANADIAN YEARS. The Story of the RAF's all-Canadian Fighter Squadron, by Hugh Halliday. 16 cm by 24 cm, 177 pages, 100 photos, 3 pages of aircraft drawings, and 5 maps. Canada's WINGS (1981). Hardbound Can\$ 16.95.

The price that the democracies pay for their habitual unpreparedness for war is the lives of their young men. No where is this lesson more clearly demonstrated than in France in 1940 and in Java in 1942. Perhaps such tragedies can be avoided, or maybe they are the price that must be paid for "freedom". In any case, it is important to understand the consequences of the various "tradeoffs" that must be made when deciding on the degree of preparedness for war than can be tolerated within a democracy. Such an understanding must be based on the study of history.

242 Squadron RAF bears the dubious distinction of being put onto the sacrificial altar twice; first in France and then on Java. However, this book, covering only the "Canadian years", stops before the unit ships out for the Far East. Perhaps the story of its operations against the Japanese will someday be covered in another book.

Officially formed on 30 October 1939 at Church Fenton, Yorkshire, the pilots were initially all Canadians who had enlisted in the RAF. It was intended that an all-Canadian squadron would dramatise the Commonwealth's support of the war. After training on Magisters, Harvards, Battles, and Blenheims, they received Hurricanes in Jan-

uary 1940 and were declared operational for day missions on 25 March and for night missions on 11 May, one day after the German invasion of Holland, Belgium, and France.

On 14 May, four pilots of the squadron were sent to France to be attached to RAF units already in action. On the 16th, "A" Flight of 242 Squadron, consisting of six pilots followed. Their stay in France was short but eventful; pilots and ground crew returned to England by the 26th but not before downing half-a-dozen enemy aircraft and suffering 40% casualties (one killed, one captured, and two wounded). Flying from bases in England, 242 Squadron began operating over France again on 22 May. From 27 May to 11 June they participated in protecting the evacuation of the British Expeditionary Force from Dunkirk. Losses during this period were serious; fifteen pilots killed, captured, or wounded between 14 and 31 May.

In his American-based ignorance, this reviewer had always assumed that British participation in the Battle of France ended with Dunkirk. That this was not the case is illustrated by the return of 242 Squadron to France on 8 June. But, it was too late. While France crumbled about them, 242 Squadron retreated from base to base, finally embarking for St. Nazaire on the Polish liner "Sobieski" on the morning of 16 June. The ground crew nearly boarded the ill-fated liner "Lancastria", but plans were changed the last minute thereby saving them the sharing the fate of the 2800 persons who lost their lives when the "Lancastria" was sunk by German bombers.

Back in England, the squadron was re-equipped in time to participate in the Battle of Britain under the command of the legendary Douglas Bader. After the Battle of Britain, the squadron went over to the offensive flying missions over occupied France until 3 October 1941 at which time they were declared non-operational and preparation was begun for transfer to the Far East. By this time, more and more non-Canadian personnel had been transferred into the squadron to replace losses so that at the time of its disbandment there were few Canadians left in the RAF's all-Canadian squadron.

As can be seen from this brief history, the story of 242 Squadron must, of necessity, be mostly a description of combat. The author describes this combat exceeding well, relying on official reports, log books, newspaper accounts, personnel letters of the time, and interviews of surviving personnel. The photo coverage is excellent; Tiger Moths, Hart Trainers, and Blenheims used in training are included, but most of the photos are of personnel and Hurricanes. Appendices include a list of Honours and Awards Granted to Members of No. 242 Squadron 1939-41 (16 awards), a list of Aircrew Personnel of No. 242 Squadron 1939-41 (including English, New Zealand, Polish, Australian, Czech, French, Norwegian, & American; P/O F. Suman and H. Szczesny are described as experienced pilots who scored at least 2 and 6 victories respectively during the Battle of Britain. No nationality is given for P/O S. Skalski, but this can be no other than the leading Polish ace of WWII.); Canadian Groundcrew Serving in No. 242 Squadron 1940-41; Representative Aircraft Flown by No. 242 Squadron (including s/n and histories, e.g., "P2732 Shot down near Dunkirk, May 31st 1940. Pilot Officer C.M. Stewart killed." and "P3088 LE-L Crashed by Pilot Officer Grassick, July 11th 1940. Later transferred to No. 249 Squadron."; and "Hawker Hurricane" 1/72-scale side-view drawings of Hurricane LS-X burned during evacuation from France, LE-F with two-bladed Watts propeller, LE-D in September 1940, aircraft of S/L Bader, LE-A the aircraft in which F/O McKnight was killed in action on 12 June 1941, and LE-S in mid-1941. These aircraft were chosen to illustrate the evolution of Fighter Command markings during this period. It is interesting to note that the emblem so closely associated with S/L Bader, a leg giving the "boot" to Hitler, was carried on most 242 Squadron aircraft.

242 SQUADRON, THE CANADIAN YEARS is a fascinating book to read and it is highly recommended to all SAFO readers interested in the Battles of France and Britain. This

book, and other publications of Canada's Wings, can be obtained from The Hanger Bookshelf, P.O. Box 1513, Belleville, Ontario K8N 5J2 CANADA.

Review copy supplied by Canada's Wings.

"DUSTBIN" VENTRAL BOMBARDIER/GUNNER POSITION FOR Ju52/3m, 1/72 scale, The Guano Aeroplane & Zeppelin Works, 5802 Northeast 59th Street, Seattle, WA 98105.

This conversion kit by Wesley Moore (SAFCH #84) consists of 5 parts (dustbin, windscreen, struts, gun mount, & MG15 machine gun) finely moulded in clear plastic. The machine gun, in particular, is a little gem; this is a shame since most modelers will mount the dustbin in the retracted position (as it had to be when the aircraft was on the ground) relegating the gun to the spare parts box. The instruction sheet warns that the plastic is very brittle, but this should be no problem once the parts are removed from their sprue as little or no work will be needed on the parts and the construction is extremely simple. The instruction sheet provides a perspective view of the dustbin as well as side and bottom views in 1/72 scale. The latter view also provide the scale drawings to scratch build the bomb doors and to position everything correctly on the aircraft.

Besides helping with the construction, the instruction sheet provides color schemes for 5 aircraft: D-ALEF of the 'secret' Luftwaffe, 1934; 27+E11, 1936; 22o76 of the Legion Kondor, 1936; an aircraft captured by the Spanish Republican, 1938; and A-703 of the Swiss AF, 1947. All these aircraft have wheel spats so the Testors/Italeri kit must be used. A nice innovation is the inclusion on the instruction sheet of all markings drawn to 1/72 scale; this will greatly facilitate either finding the correct size and style numbers in your decal box or, if worse comes to worse, making your own markings. One word of caution, however; the instruction sheet warns that "Some of the colors on the marking diagram are a little questionable, so feedback from those with better knowledge would be appreciated!", and that "If these is enough demand, it may be possible to publish a decal sheet for the aircraft on the instruction sheet (or others); Ju52 enthusiasts are invited to submit their favorites."

The price is \$1.50 each, plus \$0.40 "postage and bother" per order, and they can be obtained directly from Wesley at the address given above. Wesley feels that he will be able to "mass produce" this kit and he invites inquiries from prospective dealers. If enough interest is expressed, perhaps we can make this kit available through the SAFCH. If your not in a hurry for your kit, let me know of your interest and I'll see what can be worked out.

In summary, this is a nice little kit of a conversion that is rather more difficult than cursory examination of photos may indicate. It is highly recommended to any SAFCH modeler desiring to add a model of an early Ju 52 to their collection.

Review copy supplied by The Guano Aeroplane & Zeppelin Works.

IPMS HAWAII, 1650 Leha St., Honolulu, HI 96818. Issue #8 of this chapter's publication consists of 12 pages and 10 photos devoted entirely to the Falkland/Malvinas conflict. Air, ground, and naval equipment are covered from the modelers viewpoint, with modeling possibilities discussed and references critically examined. By their accounting, kits are available, in one scale or the other, for all the aircraft involved except for the Wessex, A-109, Nimrod, MB-339, and Learjet 35A. Ground equipment is poorly covered in kits - nothing is available in any scale. The figure builder is well off and so, surprisingly, is the ship modelers with the Airfix 1/600-scale Devonshire, Amazon, Leander, and Fearless providing the bases of such well-known ships as the Antelope and Ardent. Also included is some of the most straightforward talk about this unfortunate war that I have yet seen.

The first six issues of the IPMS-HAWAII publication

(with additional material) are available as a 50-page booklet for \$5.00.

Scale Aircraft Accessories: Those of us who were modeling airplanes (dare I say "aeroplanes") before the advent of plastic kits remember solid scale-model kits with mixed emotions. Some still shudder when they recall the patience, not to mention the talent, required to fashion a block of balsa wood into a reasonable facsimile of a fuselage or wing. And, some of us never did get the hang of filling the surface well enough so that the grain didn't show through the paint. Nevertheless, whenever "mature" modelers talk about the "good old days", these problems are soon passed over and the conversation invariably turns to pleasant memories of "those excellent" metal-cast radial engines and machine guns that were included with the better kits. I've always thought that the passage of time has endowed these items with greater fidelity than they actually possessed, but one can not dismiss the fact that any hobby shop worth the name had a good stock of cast-metal engines, etc., available at a cost of literally "pennies". This latter aspect is most fondly recalled by the older scratch builders (the adjective "older" is probably redundant in this usage), who often ask; "Why doesn't someone make metal castings for use in scratch building airplanes?"

It is pleasure to announce that someone does make parts especially of scratch building aircraft and that these parts are even better than the ones I remember. Aeroclub Models of England has available an excellent, and growing, selection of high-quality, white-metal castings for 1/72 and 1/48 scale aircraft. Included in their catalogue are propellers, engines (with propellers), ejection seats (something we didn't need "back then"), machine guns, arrestor hooks, and such miscellaneous items as tail wheels, DF loops, oil coolers, venturi tubes, & control columns. (Stores and wheels will soon be added to the list.)

The quality of these parts is outstanding. The engines, for example, are little gems suitable as center pieces of a diorama. (How about "Bench running a LeRhône somewhere in France"?). Equally welcome is the fact that the prices, while not "pennies", are still extremely reasonable: 61p - 96p (approximately \$1.00 to \$1.50) for an engine with propeller.

A partial list of items from the catalogue will give you a good idea of the range of castings available. Engines (1/72): Gypsy cowl with prop. Pobjoy and prop, 5-cyl. A.S. Genet, 7-cyl. A.S. Genet, 7-cyl. A.S. Lynx, 7-cyl. A.S. Cheetah, 14-cyl. A.S. Jaguar, 14-cyl. Panther, 7-cyl. Siemens SH14, 9-cyl. Salmson 9AD, 9-cyl. Bristol Jupiter/Pegasus, 9-cyl. P.W. Wasp, 9-cyl. Clerget rotary, 9-cyl. 80 hp LeRhône rotary, 9-cyl. 110 hp LeRhône rotary, & 9 cyl. Bentley rotary. Engines (1/48): 5-cyl. A.S. Genet, 7cyl. A.S. Genet, 7-cyl. A.S. Lynx, 14-cyl. A.S. Jaguar, DH-type cowl and prop, Cirrus-type cowl and prop, Bristol Jupiter, 9-cyl. Gnome Monocoupape, & 9-cyl. LeRhône.

Until the appearance of Aeroclub Models' Scale Aircraft Accessories, I was afraid that my scratch building efforts would be limited to aircraft with completely enclosed, water-cooled engines. (In fact, since I detest carving propellers and I haven't figured out how to scratch build wheels, I am afraid that after my meager spare parts box is exhausted, my repertoire will be limited to jet-powered seaplanes.) Now, even the novice scratch builder can tackle almost any subject with confidence that the job can be completed.

I can not recommend these accessories too highly, but you do not have to take my word for it. Send a couple dollars to Aeroclub Models, 5 Silverwood Ave., Ravenshead, Notts., NG15 9BU, ENGLAND and ask for a sample in your scale and for their catalogue. Please note that they prefer payment in Sterling.

Aeroclub Models is also producing vacuform kits. Their 1/48-scale Hawkers Hart has received good reviews in the English magazines.

"Regarding the NEIAF Moth 'D-101' described in SAFO #26: I have delved extensively into the history of the NEI Moths and Tiger Moths, but I have never ran across any carrying military serials. It is not impossible; for example I have a photo of a Piper J-4 Cub Coupe with the orange triangle markings, albeit without serial. However, I would certainly like to see the original source of this information.

"There were a few Moths in the NEI, but I doubt very much that any were ever impressed into military service. Why should they be used when more modern Tiger Moths were readily available?

"The NEI civil register for June 1939 shows four active Moths: PK-SAD DH-60G c/n 1046 (definitely no longer active by 6-40 because registration was re-issued to a Me-108); PK-SAS DH-60X c/n 412 (identity somewhat uncertain); PK-SAO DH-60GIII c/n 5042 (w/o near Surabaya 7-39); and PK-SAF DH-60M c/n 1687 (w/o date not know, but sometime between 6-39 and 1-41). Of these four, only PK-SAS was still active on the 1-1-41 register. Unfortunately, no interim civil registers are known to exist which would allow dating the write-offs more accurately. The confusion about PK-SAS is a long story which would take too much time to discuss here.

"There were two groups of Tiger Moths operated in the NEI. The first series was owned by the NILF (Nederlands Indisch Luchvaart Fonds - Netherlands Indies Aviation Fund) which assigned the aircraft to flying clubs for use by civilian members at subsidized rates.

"The NIFL initially operated four Tiger Moths imported from the UK (used aircraft bought via Shackleton Aviation), PK-SBA thru SBD; c/n 82233, 82234, 82593, & 82594 (the tie-ups are not certain). These aircraft were shipped out of the UK on 8 May 1940. A further ten were bought new in Australia: PK-SBE thru SBN; DHA c/n 145, 144, 141, 143, 410-415 respectively. In addition, there were four more purchased in Australia with funds provided by the flying clubs themselves (not NILF money), although these were purchased through NILF channels: PK-BPP, Balikpapan Flying Club, c/n 507; PK-PAL, Zuid Sumatra Flying Club (Palembang), c/n 432; PK-?, Medan Flying Club, c/n 677; PK-?, Medan Flying Club, c/n 687.

"A second series of 48 Tiger Moths were bought specifically for the VVC (Vrijwillig Vlieger Corps - Volunteer Pilot Corps). Although the latter was still a civilian set-up, the intent was much more military, with the admission of students based on suitability for military service.

"The VVC came about through an initiative of the Batavia Flying Club, which felt that the original NILF set-up was too limited. They wanted a much larger organization, with pilot training free of charge for suitable candidates. Initially the NIFL was not in favor of this, nor was the NEI Army, so the Batavia Flying Club started the VVC on its own, but continued to push for general acceptance through various influential politicians. They got the CG of the NEI Army to intervene and the VVC was officially accepted by the Army and Navy. Military instructors were assigned and the VVC was brought under the general administration of the NILF. When war broke out, the VVC students were drafted into the Army and Navy and assigned to military pilot classes.

"The code 'D-101' is very unusual. The NEI Army numbering system mentioned by Michel Ledet in SAFO #27 is essentially correct. [However, '5' was for 'horizontal bombers'; dive bombers (Brewster Bermudas were ordered but never arrived) were to be '7', and the Koolhoven FK-51 belonged in category '1' (training aircraft that could be armed). Category '2' was for transition trainers - the Lockheed 212.] For this reason, the Moths and Tiger Moths would have been put into category '0'; 'D-101' would not seem correct at all. In addition, the Moths, if they had received military numbers, should have followed the Ryan series (R-010 thru 069) with -070, etc. [There is a photo of a Ryan ST (Kinner radial) with 'R-070'. This does not have to exclude a Moth with 070, because the Ryan project office could have specified 070 before a Moth was possibly impressed. A similar situa-

tion existed with impressed Wacos which took up the already issued numbers of a follow-up order for Lodestars, obviously because this had escaped attention.] Another hitch is that the 're-numbering' order of 16 August 1940, shows the letter 'D' as used for Douglas transports (most likely used, or reserved, for a short period for the two DC-5's which were the property of the NEI government but eventually were operated by KNILM Airlines). Thus, the Moth should have been 'DH' or something similar, but not 'D'."

"Finally, concerning the Dutch Navy serials S-1 thru S-9: It is by no means certain that these were nine impressed Moths as suggested by Michel. I checked with my friend Nico Geldhof who is the expert on the MLD and he has checked just about every MLD pilot's log book still available. He has not found any mention of the S-1/9. As far as is known, the MLD leased six NILF Tiger Moths for a few hours per day (the rest of the time they were flown by the Surabaya Flying Club), between November 1940 and April 1941. These were PK-SBB thru SBB and during the period concerned they were flown with PK registrations. The MLD also borrowed a few NI Army Ryans before their own Ryans arrived, but again, these flew with Army numbers.

Jerry Casius (SAFCH #649), 91 N. Main St., Cedarville, NJ 08311

"I finally have had an opportunity to sit down and jot down a few comments on SAFO #27 which has been languishing in a pile here for lo these past four months or so.

"In the very interesting SEAR Snippets, under Bolivia, FAB F-86F serials so far reported have been FAB 651-653, 655, & 658, but photo evidence or sighting reports are needed to confirm the existence of 650, 654, 656, & 657. The latter three seem to be good assumption, but 650 is very questionable. Under Brazil, it may be more accurate to state that F-80C FAB 4228 was w/o before being taken on charge by the FAB; the exact location of its demise is not confirmed as being in Brazil. Also, is it certain that Catalinas 6551 and 6552 are actually PBX-6A's? Under Chile, the Canberras acquired are PR Mk 9's not 7's. Under Panama, the FAP Bell's are not all UH-1H's; when I saw them, FAP 104 & 105 were late model UH-1D's and 107 was a UH-1B. Under Costa Rica, the Ministerio de Seguridad Publica will be surprised to learn that their Guardia Costa received three DHC-3's (actually they were U-1A-DH's). The relationship of these aircraft to the FAP is highly questionable; anyone who has been to the area can tell you that there is little love lost between these two nations.

"The interesting letter from Armando Gil concerning the struggling FAV Museum contains several errors. The aircraft which carried out the attack on the rebels of Colonel Roman Delgado Chalbaud at the Cumana 'beach-head' were Breguet Bre.19's, not the diminutive Bre.27's, the first of which did not appear in Venezuelan colors until several years later (1933 seems about right). Historian and modelers should note that the aircraft held by the Museum are almost entirely finished in spurious color schemes and serials. The Museum's Flamingo aircraft is not a replica, but the actual aircraft reclaimed from the Venezuelan wilds where it was left by American adventurer Jimmy Angel at what was later named Angel Falls. It would be extremely helpful if the c/n of the aircraft in the Museum's collection could be established.

"I was very pleased to see Georg von Rauch's comments on the review of John Andrade's book 'Latin American Military Aviation'. Was the designation of the 20 Focke Wulf FW-44's delivered to the Argentine Army from the manufacturer really FW-44K or is this a typo? The legend of the Nieuport-Delage NiD-29C.1's in Argentine service seems to go on, and it would be a pleasure to put this matter to rest once and for all. My personal hypothesis is that the aircraft referred to were Nieuport 28's, several of which did serve with the Argentine Army. We need the help of our French readers on this issue. In addition, maybe one of our friends in France could comment on the identities of the various Dewoitine products mention-

ed in Georg's letter. My references, which include all the issues of JANES, fail to divulge the existence of two-seat versions of the D.27 or the D.25. However, this does not preclude such a variants from having been produced. Georg's comments on the D.25 suggest that the aircraft was an observation type rather than a trainer, which further arouses my curiosity.

"Several comments on Ismael Garcia Llacas's article on the 'Howard DGA-6 in Mexico': First off, the aircraft were Howard DGA-8's, c/n 82 and 83, ex NC18282 and NC-18283, which were delivered about 19 September 1937. One of the parties involved in smuggling these aircraft to Cedillo was none other than Lloyd Clevenger. Dipl. Ing. Jose Villela, the dean of Mexican aviation historians, in a letter to me of 6 May 1982, states that these two aircraft (together with a third aircraft operated by the Cedillista Fuerza Aerea, a Spartan 7W c/n 7W-5, ex NC-13998 which later became XA-CFA) were absorbed into the FAM but later sold to Spain for the civil war there. Unfortunately, Patrick Laureau's excellent book, 'L'Aviation Republicaine Espagnole', gives no clue as to the subsequent fate of these well-travelled aircraft. Like so many aircraft of that era, I seriously doubt if any of these ex-Cedilla aircraft ever actually bore FAM service markings or if they were actually operated by the FAM.

"Ron Andrini's effort on the FAS Ouragon was a good stab at a difficult subject to document. However, the rudder stripes did not extend all the way to the base of the fin, and the presentation of the s/n is too small. I have supplied Nick Waters with information on one of these aircraft, '707', which will probably appear in the SAFO.

"In closing, perhaps SAFO readers will be interested in the fact that John Dienst and I have a book in preparation on the North American P-51 Mustang aircraft in Latin America. Any reader who feels that he has something to contribute on this subject should drop me a line."

MSG Daniel P. Hagedorn (SAFCH #394), 290-40-0270, HHC 3d Brigade, APO New York 09074

"Enclosed is a drawing of an Algerian Fokker F.27 seen at Le Bourget Airport while on its way to the parking area for UTA Industries. This aircraft didn't stay long for it was gone two days later (28 May 1983).

"Armando Gil's letter in SAFO #27 on the Venezuelan Air Force Museum at Maracay prompts the following remarks: I visited this museum in January 1980 at which time I saw an Umbaugh U18 autogyro in Venezuelan colors. Has anyone any idea of what role this aircraft might have played while in military service? There were two Ryan Flamingos at the museum; one was the replica mentioned in SAFO, but the other was the original aircraft Jimmy Angel was piloting when he discovered 'Salto Angel'. In the 1970's FAV personnel salvaged this aircraft and brought it back to Maracay. Finally, the Mustang has never seen FAV service; the aircraft on display was a gift from the Bolivian Air Force in 1976 and the museum staff put it into FAV colors.

Jean-Paul Garcia (SAFCH #573), 82 Rue de l'Egalite Apt. 47, 93260 Les Liles, FRANCE

"I have some information from Switzerland that might be of interest to our readers:

"Two new Mirage two-seaters have been delivered to the Swiss Air Force; Mirage 5DS 'J-2010' and 'J-2011'. The 18 recce Mirage IIIRS are to receive small canards (similar to the Mirage 4000) and rebuilt wings by 1984/5. (The interceptor Mirages and the two-seat IIIRS will remain unchanged. The interceptors are scheduled to be replaced by 1990 whereas the Mirage IIIRS's will remain in service for twenty more years.)

"Three UH-60 Blackhawk helicopters have been ordered to replace the three Ju-52's which now belong to a civil foundation and are available for chartered pleasure flights.

"I am very interested in collecting and trading color slides of military aircraft, and I am looking for correspondents in Central and South America, Africa, and Asia. I am also interested in Scandinavia.

"Has anyone information about the Lockheed U-2 and the RB-57 of the Nationalist Chinese Air Force? At least four of the former were flown with Chinese markings. Any bit of information would be helpful.

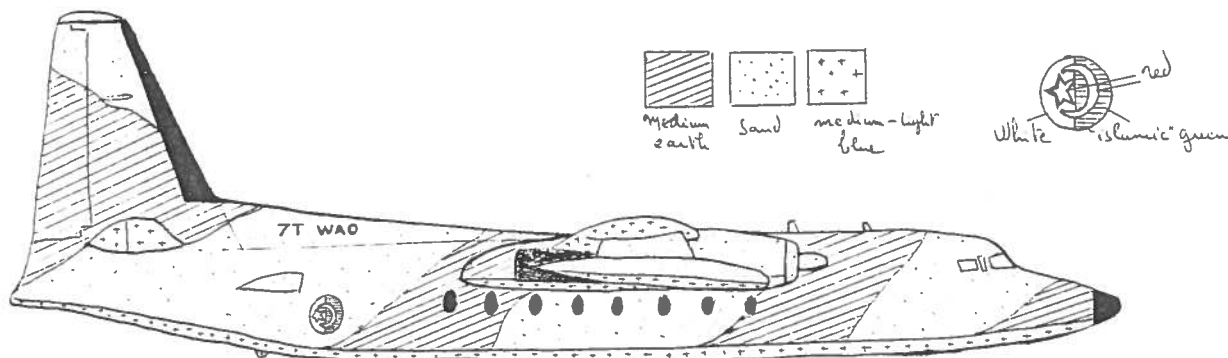
"I am doing research on the Indonesian Air Force and I would like to correspond with any readers who have information about Indonesian MiG-15, MiG-15UTI, MiG-17, MiG-19, MiG-21, F-51D, B-26, Il-28, B-25J, Tu-16, An-12, DHC-3, C-47, Il-14, C-130, JetStar, Cessna 401A, 402A, Turbo Skywagon, PZL-104 Wilga, Alouette III, Bell 204, Mi-4, Mi-6, S-61A, L-29, T-6, Jak-11, and any other types that flew in Indonesian colors. In naval service were MiG-19, MiG-21, HU-16D, PBX-5A, Alouette II and III, Bell 47, Sikorsky S-58, C-47, Nomad, Bo-105, Wasp, and a few Cessnas."

Michael Grunenfelder (SAFCH #627), Dufourstrasse 19, 9008 St. Gallen, SWITZERLAND

"Further information has come my way regarding the BAE Hawks in the TNI-AU (see SAFO #25); 17 aircraft have been delivered with the 18th on the production line and 3 more on order (making a total of 21).

"Additional information on Gibbs Sepik Airlines and their Norsemen (see SAFO #22): The tail checks on the Norsemen were a very dark green and not black (b&w photos can be hard to interpret). Gibbs Sepik Airlines had 5 Auster J5's (VH-KSD, KSQ, KSS, KSU, & KST), 9 Norsemen (VH-BNE, BNL, BLM, BNT, BHG, ASS, ASR, ASN, & GSA), and 3 Junkers Ju52/3m's (VH-BUU, BUV, & BUW). The Norsemen were re-engined from their original P&W R1340-AN1 to P&W R1340-S1H1-G geared engines taken from surplus CAC Wirraways and used 3-bladed propellers. These Norsemen were all ex-RAF. The Ju52's were ex-SAS (registration unknown) and were pretty well beaten about. After Bobby Gibbs flew VH-BUU from Sweden to New Guinea, he had it re-engined from the original BMW B2Z-3 engines to P&W R1340-S1H1-G, again taken from surplus Wirraways, but this time the Wirraway cowlings were used as the original cowlings were too narrow for the new engines. The other two Ju52's were re-engined in Sweden by GSA engineers and then flown to New Guinea by GSA pilots."

Mick Mirkovic (SAFCH #465), 11/32 Curkewis St., Bondi, NSW 2026, AUSTRALIA



"I have some news on CAF aircraft, as well as some locally produced products for the modeller, which I'd like to pass on to SAFO readers.

"First, there has been some significant changes in the national markings carried by CAF aircraft. All camouflaged aircraft, including helicopters, now carry low-visibility Canadian national markings. There are three variations: (1) two-tone blue markings carried on the CF-18; (2) blue and red roundels (minus white background) carried by ground-attack aircraft such as the CF-104 and CF-5A; and, most recent, (3) black maple leaf in a black roundel carried by three-tone grey CC-130 and all camouflaged helicopters, i.e., Chinooks, Hueys, and Kiowas. As far as I can tell, location and size of the roundels remain the same.

"Helicopters which are not camouflaged, e.g., rescue aircraft, retain the standard full colour national markings. I do not know if this toning-down extends to the Sea Kings. The markings on the CF-101 remain unchanged. Maritime aircraft such as the Tracker and Auroras also retain the old-style markings. [A note on the Aurora color scheme: the grey on the lower 3/4th of the aircraft is not the same gull grey used on USN P-3s. The Canadian grey is actually a green-grey similar (identical?) to the green-grey on Trackers and Sea Kings. Colour photos of the Auroras can be deceptive since this hue does not photograph well. This is an effect similar to the grey-green on the old two-tone A-10 scheme.]

"There are several Canadian companies producing decals for Canadian aircraft. Flight-Colors set 11-72 offers markings for the CF-18 (2 complete aircraft) and the three-tone grey CC-130 with black national markings (C\$6.49). A new company, WMB Graphics, offers sets for the CF-18 which allow you to finish two aircraft; these decals come in 1/72 (C\$2.99), 1/48 (C\$3.99), and 1/32 (C\$4.99). Sabre Decals offers two sets of black numbers #9-72 for new-style CAF numbers and #10-72 for the old-style numbers. Each set contains two sheets of numbers for C\$2.99. Finally, Can-Force Decals is offering five new sheets: (1) old-style maple leaf roundel with Union Jack tail markings; (2) new-style roundel (not low-viz) and tail 'flag'; (3) fuselage red-and-white stripes; (4) old-style Royal Canadian Air Force logo; and (5) new CAF logo and Armed Forces/Forces Armees markings (without roundel). Each set is for 1/72 and sells for C\$4.95.

"A book which might interest SAFO readers is *Modern Military Aircraft and their Markings, Vol. 1 Canadian Armed Forces*, by Douglas and Phillip Tachauer. More a magazine than a book, this volume collects 31 pages of b&w photos of contemporary Canadian aircraft. The descriptions of the colours are not great: 'the aircraft is grey and green' is a typical example, but more effort goes into describing squadron emblems. The book sells for US\$4.95 (C\$5.95) plus postage; from Douglas R. Tachauer Aviation Publications, 1608 Robillard Rd., Mississauga, Ontario, L5J 3K7 CANADA.

"All the decals mentioned above, plus many other Canadian products (e.g., 1/72 vacuform kit of the CF-105 Arrow) are available by mail order from Hobby House, 478 Rideau St., Ottawa, Ontario, K1N 5Z4 CANADA. They accept payment via credit cards. They also carry the Compucolor line from England."

Michel Barnes (SAFCH #502), 569 Dufferin St., Toronto, Ontario, M6K 2B1 CANADA

"I have some information about some unusual kits that would be useful in making models of aircraft (and other things) that fought in the Spanish Civil War.

"1. I have just received from Wesley Moore (SAFCH #84) of Guano the long-awaited Ju 52 3m/g3e 'dustbin' conversion kit of five clear injection-molded parts costing \$1.50 plus about \$0.40 US postage and about \$0.70 European postage. The kit is excellent and includes a template showing the positioning of the dustbin and bomb doors; the latter have to be scratch built. There are also five color schemes: 2 Luftwaffe, one Legion Condor, one captured Spanish Republican, and one Swiss. Insignia are given in 1/72 scale so they can be xeroxed directly

onto an ATP decal sheet.

"2. Bill Brown (SAFCH #526) has heard rumors that a resin-cast kit of the Loire 46C1 in 1/72 scale is almost ready for release. This aircraft was used by the 'Segunda Lafayette' of the Spanish Republican Aviacion Militar.

"3. According to an ad put out by The Arsenal (of Nebraska) a resin-cast kit of the Romeo Ro-37bis (radial engine) is already on the market.

"4. For SCW diorama freaks, CZECHMASTERS produce 1/76-scale resin-cast kits of the Fiat-Ansaldo CV-33 or L-3/35 tankette, the Soviet T-26B tank, and an early version BT-7 tank convertible to the BT-5 used in Spain.

"5. Paul Whelan (SAFCH #553) reports seeing the resin-cast conversion to make the Heller Nieuport-Delage 622 into the Hispano-Nieuport 52C1. He says the kit is expensive, but not how much.

"6. There already are CZECHMASTER epoxie kits in 1/72 scale of the Polikarpov I-15 and I-152 on the market, with Airmodel planning to release a double vacuform kit in the same scale.

"7. I have still not heard of any release dates for the Kozovavody Prostejov (Avia)/Bloch MB 200, Aero A-100 (convertible to the A-101), and Fokker F.IX, all in 1/72 and injection molded."

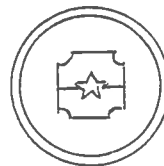
Tom Sarbaugh (SAFCH #497), 25 Roble Rd., Berkeley, CA 94705

"Due to many long standing and increasing personal problems, I would like to inform my correspondents that for the foreseeable future I will have little time to answer letters. Only urgent matters will receive a prompt reply. I promise that as soon as possible I will return to full activity.

"At this time I would like to provide a little information on national insignia that should be of interest to SAFO readers: AIR INTERNATIONAL has published an update of the world's insignia; the June 1983 issue shows Afghanistan to Haiti, the July issue Honduras to Mauritania, and August Mexico to Zimbabwe. All very interesting but not without faults (incorrect shape and/or color) and some insignia are old and other have never existed.

"A US reader has provided me with a clipping from the 6 June 1983 issue of NEWSWEEK showing a two-tone green Mi-8 with black lettering on the tail boom "FUERZA AEREA SANDINISTA" and the latest national insignia. This insignia consists of a chrome yellow disk thinly outlined in black inside a vertical rectangle equally divided horizontally red over black; the four corners of the rectangle are curved inward (see sketch); and a yellow 5-pointed star is in the center."

Fritz Braun (SAFCH #220), Posener Strasse 20, D-8000 Munchen 81, WEST GERMANY



"My pen pal in Belgium needs photos of F-5E in Singapore and Mexican markings. In addition, he would like to have information on the cockpit and interior of the H-53 Sea Stallion."

Clifton Roberts (SAFCH #636), 160 Cloverdale, Searcy, AR 72143

"Are the following 1/72-scale kits still available: Eidia Cessna 172 & Bonanza V35, Aurora Jet Commander, Monogram Twin Mustang & Huey Cobra? If so, could someone provide the address of a mailorder firm from which they can be ordered?"

David Hill (SAFCH #640), 74 Tixall Rd., Littleworth, Stafford, Staffordshire, ST16 3UA ENGLAND

TNI-AU PART 5

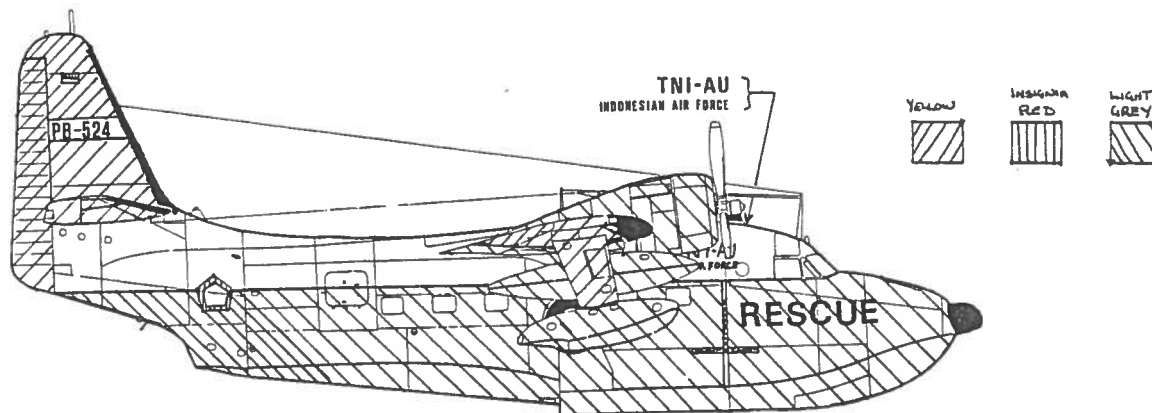
Grumman HU-16B, PB-524, pre-1980.




Nonstandard SAR markings: Overall light grey except for the following: top of the fuselage and fin stripe are white; float struts and tips of the wings, fin, rudder, & tailplane are yellow; stripe on fuselage, serial number,

and all lettering are black. Standard national markings and propeller warning bands.

Reference: MILITAIR 1982 by J. Andrade.

N.B. This drawing was prepared from a b&w photo and there may be some minor errors in interpreting the colors.

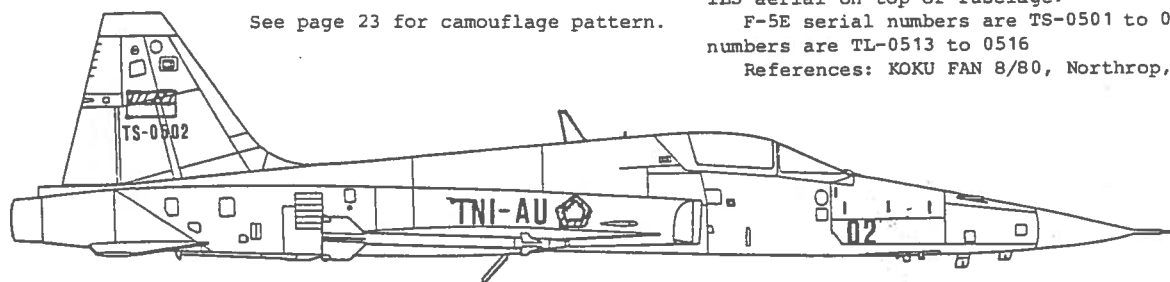


Grey (FS 36251) 
 Blue (FS 35237) 
 Grey (FS 36307) 

Northrop F-5E, TS-0502, 14 Squadron, Madiun AFB, Java. Overall wrap-around camouflage scheme of grey (FS 36251), blue-grey (FS 35237), and grey (FS 36307). Radome, serial numbers, and inscription (TNI-AU) on the fuselage and upper surface of the starboard wing are black. Standard national insignia in 4 positions. N.B. ILS aerial on top of fuselage.

F-5E serial numbers are TS-0501 to 0512. F-5F serial numbers are TL-0513 to 0516

References: KOKU FAN 8/80, Northrop, and George Kamp.



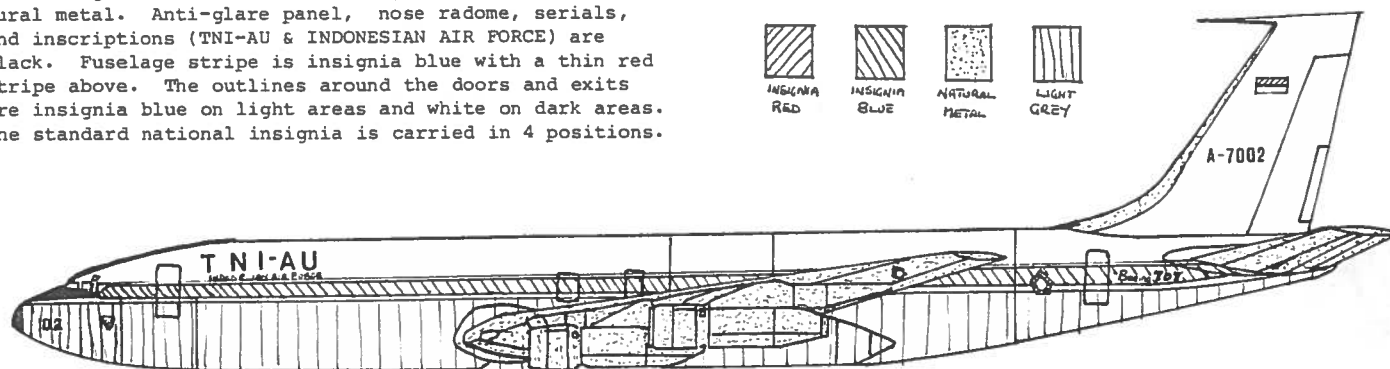
See page 23 for camouflage pattern.

Boeing B.707-3M1C, A-7002 (c/n 21092, ex PK-PJQ). This aircraft was transferred from GARUDA to the TNI-AU in late 1982.

The top of the fuselage and the fin are white; the lower part of the fuselage is light grey. Wings, tailplane, engine nacelles, and leading edge of fin are natural metal. Anti-glare panel, nose radome, serials, and inscriptions (TNI-AU & INDONESIA AIR FORCE) are black. Fuselage stripe is insignia blue with a thin red stripe above. The outlines around the doors and exits are insignia blue on light areas and white on dark areas. The standard national insignia is carried in 4 positions.

(The badge under the cockpit could be that of 31 Squadron which is the main transport unit in the TNI-AU, but the photo doesn't show it clearly enough to be certain.)

Reference: b&w photo in KOKU FAN 5/83



Mike Mirkovic (SAFCH #465), 11/32 Curlewis St., Bondi, NSW 2026, AUSTRALIA

TIGERS TOO

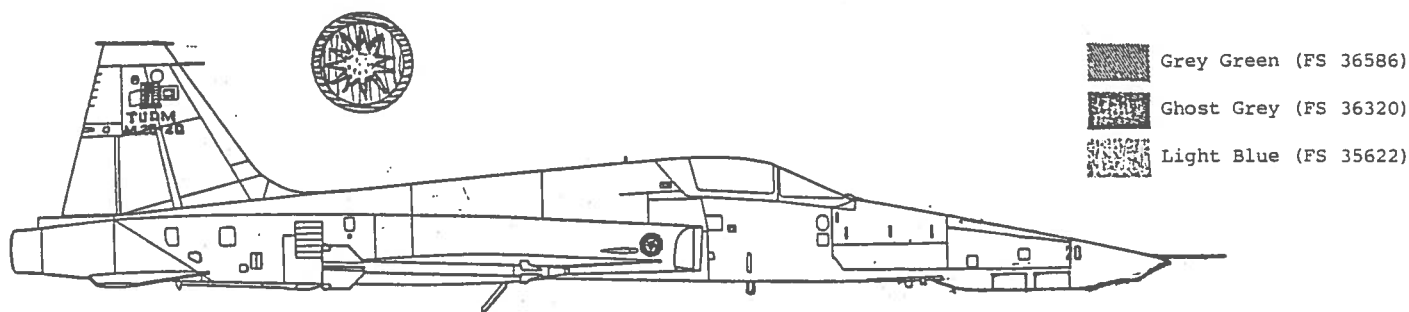
Northrop RF-5E, M29-20, Tentera Udara Diraja Malaysia (Royal Malaysian Air Force).

Overall wrap-around camouflage scheme consisting of grey (FS 36586), grey (FS 36320), and light blue (FS 35622). Serial numbers and inscriptions (TUDM on the fin and on the upper surface of the port wing and lower surface of the starboard wing) are black. N.B. Beside the photo nose, other modifications include: ILS/VOR aerial on top of the fin, F-20-type LERX (leading-edge root ex-

tensions), pitot moved to leading edge of the fin, and removal of the port 20-mm cannon.

A new serial system and a new national insignia have been introduced in 1983. The fin flash is yellow, dark blue, & light blue (rear to front). The new roundel, consisting of a yellow 12-pointed star on a light blue field with a dark blue outer ring, is in the same 4 positions as used by the USAF.

Reference: KOKU FAN 3/83 and 5/83; AIR INTERNATIONAL 6/83.



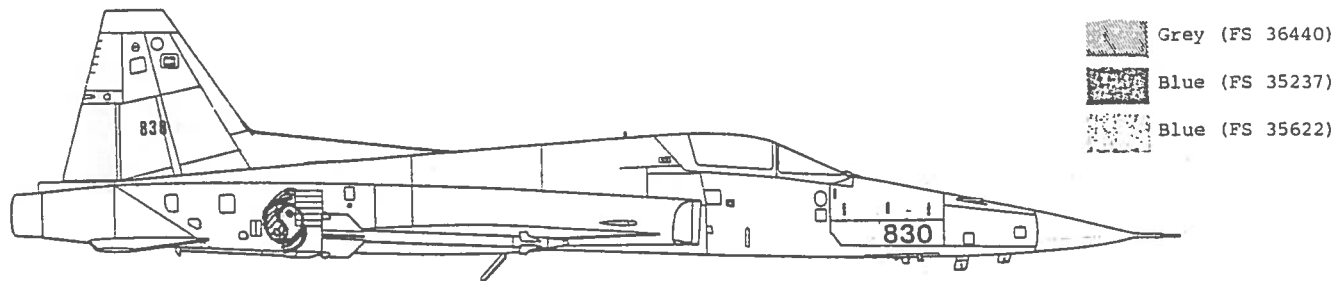
Northrop F-5E Tiger II, 830, 144 Squadron, Republic of Singapore Air Force, Tengah AFB, 1983.

Overall wrap-around camouflage scheme consisting of light gull grey (FS 36440), blue-grey (FS 35237), and light blue (FS 35622). The radome and serial numbers are black. The red and white national insignia is carried

only on the fuselage. N.B. Dorsal fin extension.

It is believed that 21 F-5E's and 3 F-5F's have been delivered to the RSAF. Known F-5E serial numbers are: 800, 801, 803, 804, 813, 814, 820-824, and 830-833. F5F serials are 850-852.

References: KOKU FAN 5/83, Northrop, MILTECH #25 Aug/Sept 81



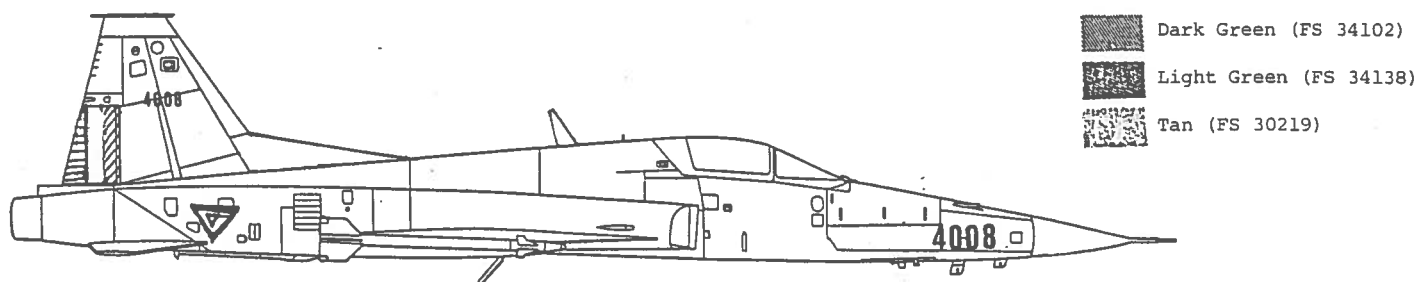
Northrop F-5E-10, 4008, Escuadron Aereo 401, Fuerza Aerea Mexicana, Santa Lucia AFB, 1983.

Overall wrap-around camouflage scheme consisting of tan (FS 30219), light green (FS 34138), and dark green (FS 34102). The serial numbers are black. The standard red-white-green national insignia (red outermost) and rudder stripes (green foremost) are carried. N.B. These F-5E's have the dorsal extension, ILS aerial on the fuse-

lage, an ILS/VOR aerial on top of the fin, and the LERX (leading-edge root extension) and the flatter shark-like nose of the F-20. (It should be noted that 4008 is the 1000th F-5E built.)

FAM serials are 4001 to 4010 for the ten F-5E and 4501 & 4502 for the two F-5F.

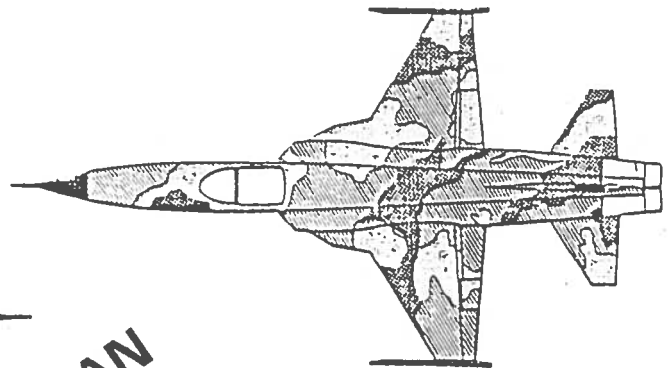
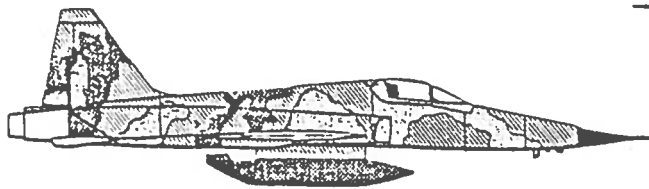
References: KOKU FAN 7/83, Northrop, MILITAIR 1982 by J. Andrade.



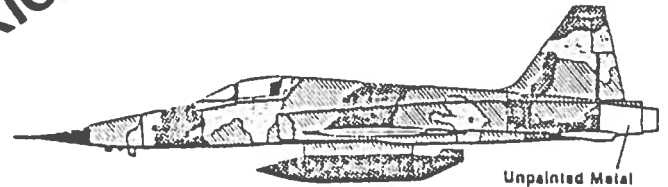
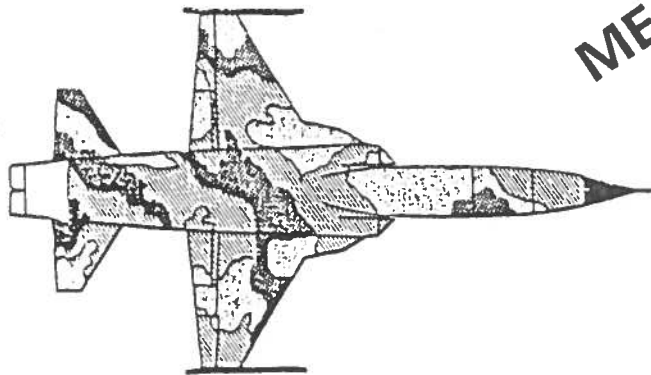
See page 23 for camouflage pattern applicable to all three aircraft.

Mike Mirkovic (SAFCH #465), 11/32 Curlew St., Bondi, NSW 2026, AUSTRALIA

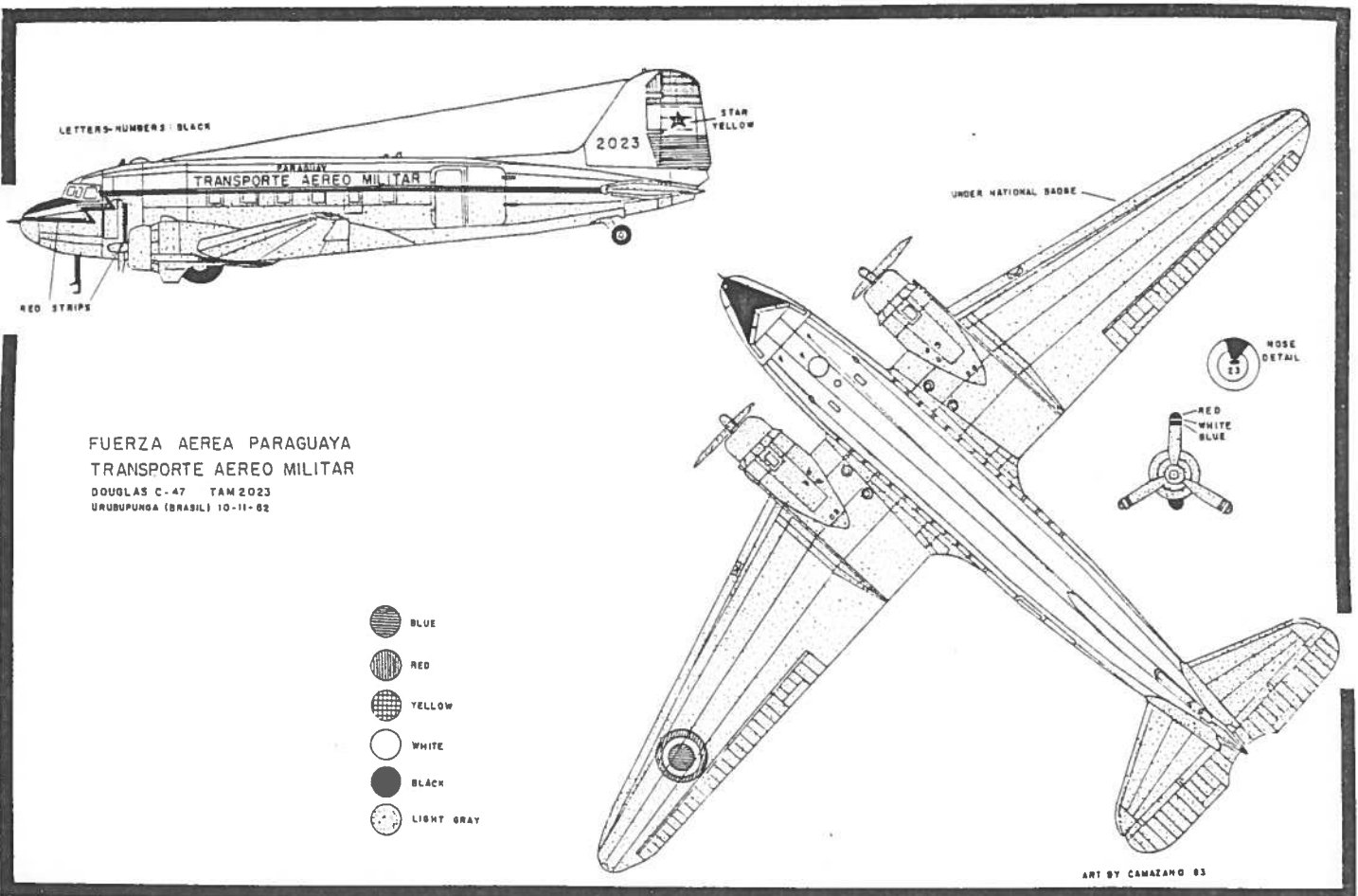
F-5E TIGERS



MEXICAN



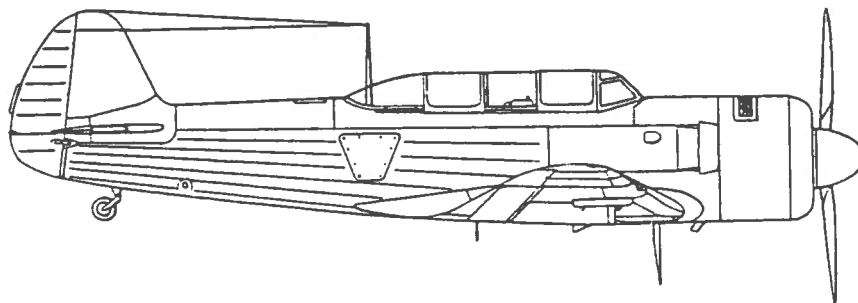
MALAYSIAN



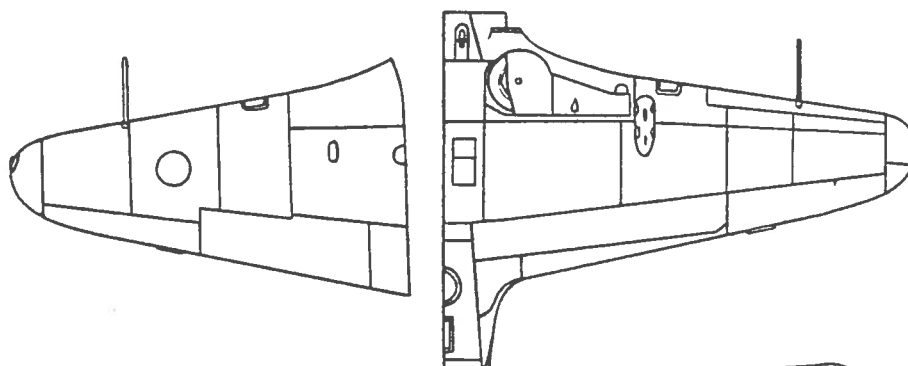
MAI
MODEL - AIRE INTERNATIONAL
MAI

YAKOVLEV YAK.II/YAK.IIU

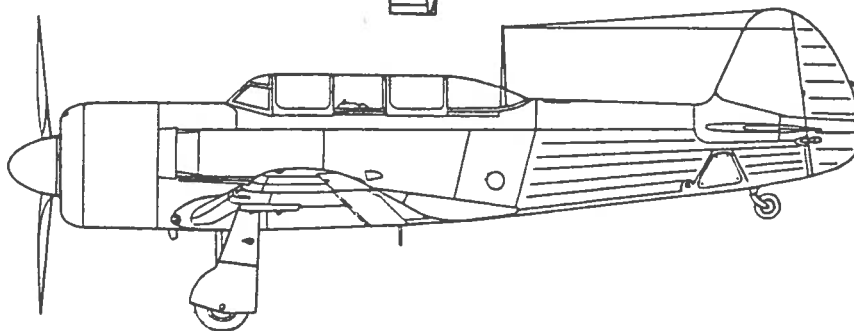
Right Side



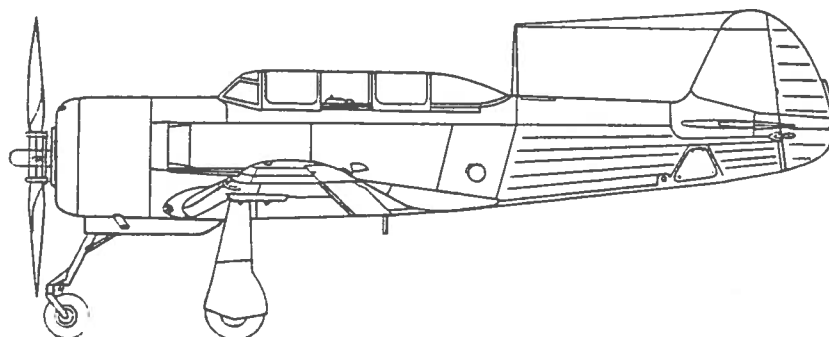
Wings



Left Side



Yak.IIU



SAFCH RESEARCH PROJECT

YAK.11 MOOSE

Thomas A. Young, PO Box 159, Olema, CA 94950

Tom Young, of Model-Aire International, is researching the the color schemes and markings used on the Yak.11. The results will be published in SAFO and as a pamphlet to be supplied with Model-Aire International's 1/72-scale resin-cast kit of the Yak.11/11U. He already has the basic references such as TBU #76; Scale Models Nov. '82; IPMS/Austria Panorama 1-2 '80; plus pilot's manual and some material from Eastern European publications.

Tom already has some color schemes for the following countries (but more is needed): Soviet, civil, Czech, Austrian, Chinese, Egyptian, East German, Hungarian, Korean, and Polish. He has nothing on the following countries: Afghanistan, Albania, Algeria, Bulgaria, Cuba, Finland, Indonesia, Iraq, Israel, Mali, Mongolia, Pakistan, Rumania, Somalia, Syria, Vietnam, Yemen, and Guinea. He particularly needs information on the markings for the Yak C.11U tricycle-gear variant.

All SAFCH members are encouraged to help Tom by sending a list of what they have on this subject to the above address.

"Here are the latest findings on Svenska Aero Brandenburg / Caspar S.I / HE1 and HE 4. (See SAFO #26 for pervious correspondence on this subject.)

	c/n	s/n		c/n	s/n
1921		31	1924		42
1922	J	32			43
1923	K	33			44
	L	34	1925		45
	M	35	1926	U	46
	O	36		246	47
	P	37			Latvia
	Q	38			Latvia
	R	39			
	S	40			
	T	41			

"s/n 31 consisted of parts built by Caspar and assembled by Svenska Aero. s/n 32 to 35 consisted of parts built by Caspar and assembled at the Swedish Navy Shops. s/n 36 to 41 consisted of parts built by Svenska Aero and assembled at the Swedish Navy Shops. s/n 42 to 44 were built and assembled by Svenska Aero. s/n 45 was probably built by Heinkel. s/n 46 was built at the Swedish Navy Shops. s/n 47 was Heinkel built. The two Latvian aircraft were probably built by Svenska Aero."

Lennart Andersson (SAFCH #68), Liljegatan 9A, S-753 24 Uppsala, SWEDEN

Argentine Naval Aviation

"Many of the aircraft frequently mentioned as serving with the Argentine Navy never actually saw service with this air arm. Such myths as the Argentine Hellcat, C-46, and Kingfisher are repeatedly mentioned. I would, therefore, like to provide SAFO readers with a list of aircraft that served with the Aviacion Naval Argentina from its origin to 1981. Some of the early types (Farman, Rumpler, Taube, Nieuport) saw service for only a short time. A few were received as gifts and some (IA-22DL & Guarani) were received on lease from the Air Force for evaluation trials.

Henry Farman	3	Douglas Model 116 Dolphin	1	Grumman F9F Panther (16)*	24
Maurice Farman	3	Curtiss 16E Trainer	28	Boeing Stearman PT-17/N2S	60
Levy GL-40HB-2	1	Curtiss CT-32 Condor	3	North American SNJ-5C	30
Rumpler Taube	1	Vought V-65F Corsair	12	Sikorsky UH-34	1
Nieuport Model XII	1	Vought V-142A Corsair	14	Lockheed Neptune MR Mk-I	8
Fokker VLC	1	Stearman 76D-1	16	DHC-2 Beaver (18)*	2
Macchi-Nieuport M-7	2	Martin 139W	1	Grumman S-2AF-1 Tracker (25)*	7
Macchi-Nieuport M-9	2	Martin 139WAN (9)*	12	Grumman HU-16B Albatros	4
Curtiss HS-11 Seagull	1	Grumman G-20 (21)*	8	Lockheed P2V-5F Neptune	4
Curtiss F5-L	6	Grumman G-15 (25)*	7	FMA IA-50 Guarani II	1
Curtiss HS-2L	12	Consolidated P2Y-3A	6	I.Ae 22 DL	1
Curtiss JN-4D Jenny	4	Stinson SR-10 Reliant	2	DHC-6 Twin Otter (18)*	1
Avro 552	12	Waco EQC-6	1	Sud Aviation T-28 (23,26)*	65
Vickers 84 Viking IV	4	Lockheed L-10E Electra	1	Beechcraft C-45H	15
Keystone Petrel	10	Focke Wulf FW-44J	7	Beechcraft UC-45J	2
Fairey IIF Mk-IIIM	6	Supermarine Walrus	10	Beechcraft RC-45J	2
Fairey IV Seal	1	Luscombe 8E Sylvaire	3	Grumman TF-9J Cougar (14)*	2
Southampton Mk-III	6	Beech D-18S	1	Beech 80 Queen Air	5
EKW Dewoitine D-21C.1	9	Grumman G-21 Goose	3	Beech 200 Super King Air	8
Savoia Marchetti S-57	1	Grumman JRF-6	3	Hawker HS-125 Srs 400	1
Savoia Marchetti S-59	1	Consolidated PBV-5A Canso	15	AerMacchi MB-326GB	8
Savoia Marchetti S-59bis	9	Douglas DC-2-267	5	McDonnell Douglas A-4Q (28)*	16
Vought O2U-1A	4	Vultee SNV-1/BT-13A	30	Beechcraft T-34C-1 Turbo	15
Consolidated PT-3	10	Grumman J2F-5/J2F-6 (21)*	32	AMD/BA Super Etendard	16
CMSA Do J Wal	5	Beechcraft AT-11 Kansan	13	SA-316 Alouette III (18)*	14
SE-5A	1	NA AT-6A/B/C/SNJ-3/SNJ-4	94	Sikorsky S-61D.4 (18)*	5
Keystone K-42A Pelikan	40	Sfreddo y Paolini SP-38	10	Westland Sea Lynx (18)*	2
Fokker Super Universal	1	Douglas DC-3/C-47 Skytrain	16	Lockheed SP-2H Neptune	4
Fairchild 45	3	Douglas DC-4/C-54 Skymaster	7	Grumman S-2E Tracker (25)*	6
Fairchild 65	1	Bell 47D (18)*	6	Fairchild PC-6B/H2 Turbo	4
Fairchild 71C	1	Bell 47G.4A (1)*	3	Lockheed L-188PF Electra II	3
Fairchild 82D	2	Sikorsky H-19/S-55 (18)*	12	Fokker F-28 Mk 3000	3
Junkers W-34hi (22)*	1	Martin PBM Mariner	8		
Consolidated 17 Fleetster	1	Vought F4U Corsair (18)*	26		

* The numbers in parentheses identify issues of the SAFO containing information on the aircraft in Argentine service.

Jorge Nunez (SAFCH #395), Humboldt 2763/65, Bahia Blanca 8000, Buenos Aires, ARGENTINA

SAFO #29 October 1983

Camouflaged Bf 109 in Spain

In July 1937, as the Battle of Brunete was drawing to a close, Fighter Group J/88 of the Legion Condor was stationed at Leon where they received 22 desperately needed Bf-109B-2s. These aircraft would not only make up for losses experienced by 2.J/88 (which had received their Bf 109B-1s in March), but would also allow 1.J/88 to retire its old, combat-weary He 51s and convert to new Bf 109B-2s. Assembled as quickly as possible, these new aircraft received numbers 6-20 through 6-41 (1). It was not long before 1.J/88 scored its first victory with these new aircraft; flying aircraft 6-36, Harro Harder destroyed a SB-2 over Huesca on 27 August 1937 (2).

Legion Condor Headquarters must have expressed an urgent need for these aircraft because they arrived in Spain wearing the camouflage recently designed for Luftwaffe home fighter units: two greens 70/71 splinter camouflage with blue 65 lower surfaces. This scheme, just stating to be used in Germany, had been designed for the forests and greenery of the homeland (3). At first, these colors presented no difficulty because J/88 was participating in the fighting for Santander and the Asturias provinces. Here, in the last of the Republican-controlled territory in the north of Spain, the vegetation was fairly lush and green. When, in late October, 1.J/88 was shifted to the fighting on the central front, they must have found that this dark green camouflage did not blend in well with the more sparsely vegetated surroundings. Therefore, the aircraft were repainted in more suitable colors. On most, the fuselage was repainted green 62 upper surfaces and blue 65 lower surfaces while the wings and horizontal stabilizers were painted in green 62 and grey 63 in a style similar to the splinter pattern. The wing tips were white. Aircraft that arrived later had their wings repainted brown 61, green 62, and grey 63. Examples of this latter scheme are aircraft 6-42 and 6-51 (4). A few aircraft seem to carry other colors. Aircraft 6-20, flown by Lt. Fritz Awe, appears to have been painted brown 61 on all upper surfaces (5). At this time, the exhaust panels were painted a matt black, similar to the earlier B-1 models.

Almost all of the odd schemes that have been illustrated in various publications depict aircraft of 1.J/88 which can be identified by its unit emblem, a small white cross within the black fuselage disk.

At least two aircraft of 2.J/88, numbers 6-29 and 6-30, are known to have been received in the two greens 70/71 splinter camouflage scheme (6) and there were undoubtedly others (7). However, 2.J/88 seems to have favored grey 63 as used on its earlier B-1 models, but with blue 65 on the lower surfaces. A photo of 6-29 shows these colors (8).

A subsequent shipment of 15, or so, more 109s was received by December 1937. These aircraft, given numbers from 6-42 to 6-56, were required to make up for losses. These losses must have been substantial because both 1.J/88 and 2.J/88 continued to use some of the earlier B-1 models and 3.J/88 could not be re-equipped with 109s until mid 1938. An example of this situation is the Bf 109B-1 (number 6-15 flown by F.W. Otto Polenz) which was forced down and captured on 4 December 1937 still wearing an overall grey 63 scheme. This aircraft had served with 2.J/88 but at the time of its capture it was with 1.J/88. This indicates that it either had been assigned to a storage or repair depot or it had been transferred to 1.J/88 upon receipt of newer aircraft by 2.J/88 (9). This latter practice must have been common (10).

At least some of the aircraft in the second shipment, if not all, were also received in the two greens 70/71 splinter scheme as photos, notably of 6-42, indicate (11). While those aircraft assigned to 2.J/88 were repainted in the grey 63 blue 65 scheme favored by that staffel, those given to 1.J/88 were repainted in the more unusual, but considering the surrounding, more practical (12) camouflage of green 62 fuselage with brown 61, green 62, and grey 63 wings as mentioned above (13). Obltn. Wolfgang Schellmann, who took over command of the 1.J/88

in December 1937, was flying 6-51, one of the Bf 109B-2s of the last batch, when he scored his first of 12 victories, an I-16 on 18 January 1938 over Teruel (14).

Later in 1938, most likely after the re-equipment of 3.J/88 with Bf 109 in June, Legion Condor Command must have decided to standardize color schemes on all their fighter aircraft. There is photographic evidence of 1.J/88 aircraft with all grey 63 upper surfaces and blue 65 lower surfaces prior to the time that they re-equipped with the newer D and E-1 models. (15) In any case, by the time these B-2s were handed over to the Spanish, all had been repainted in the 63/65 scheme (16).

NOTES

1. See Ries & Ring, *LEGION CONDOR*, Mainz 1980, p 83 bottom. On 7 July 1937, there were 17 Bf 109B-1s, numbers 6-3 to 6-19, in Spain.

2. This was Harder's second victory in Spain.

3. For good illustrations of this camouflage pattern, see: Ries & Ring, *LEGION CONDOR*, Mainz 1980, p 92 & 262; Cross & Scarborough, *MESSERSCHMITT Bf 109 VERSIONS B-E*, London 1972, p 11 & 13; Green, *AUGSBURG EAGLE*, Garden City 1971, p 24 & 26; Merrick, *LUFTWAFFE CAMOUFLAGE & MARKINGS 1935-1945 Vol. 1*, Melbourne 1973, p 70 & 49; Nowarra, *THE MESSERSCHMITT 109, A FAMOUS GERMAN FIGHTER*, Fallbrook 1963, p 22.

4. These aircraft were apparently not repainted too well, as photos show the darker greens 70/71 beginning to show through after some use.

5. See Ries & Ring, *LEGION CONDOR*, p 149.

6. Ibid. p 87 bottom & 262 top.

7. Ibid. See photo on 6-32 on p 112. The darker greens are now starting to show through the lighter grey 63 on this aircraft of 2.J/88.

8. Ibid. p 70 center.

9. If it had been transferred earlier, would not 1.J/88 have changed its color to match the rest of their planes? For this reason, I believe it came later to the unit.

10. When Obltn. Werner Molder's 3.J/88 was switched over to 109s in June 1938, the aircraft he flew for a short time was 6-7 (an aircraft which has been extensively repaired). Another member of this staffel, Lt. Josef Fozo, flew 6-15. Both these aircraft had the new, stylized numbers applied to them and both had originally been used by 2.J/88. See Fozo, *FREIE JAGD VON MADRID BIS MOSKAU*, Berlin 1943.

11. See Reis, *DORA KURFURST UND ROTE 13 Vol. 1*, Mainz 1964, p 35 center. In this photo of 6-42, the 70/71 splinter scheme is just visible. A dark 109 is in the rear, and a normal grey 63 machine is just behind 6-42.

12. "Practical" in as far as these colors are similar to dark greens 70/71.

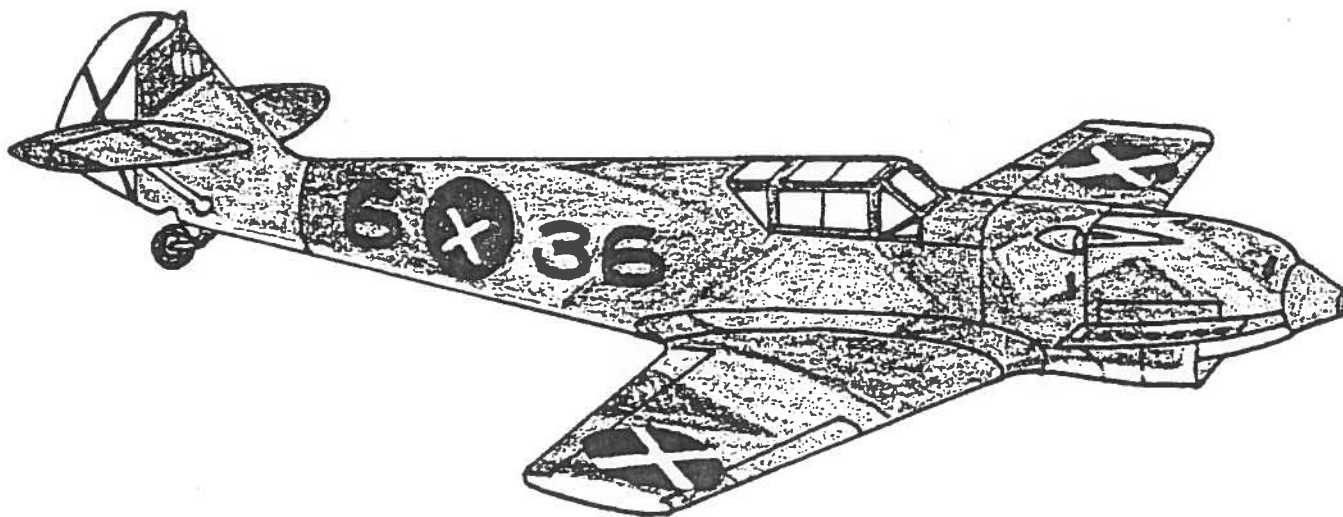
13. See Van Ishoven, *MESSERSCHMITT Bf 109 AT WAR*, London 1977, p 26 & 30 for some good photos of this.

14. Since photos of Handrik's aircraft, 6-56, show that wing guns were not carried, it must have been a B-2 and not a D as many publications have said. As a matter of fact, the earliest 109 I have been able to find photos showing wing guns is 6-60 of 2.J/88 flown by Uffz. Herbertt Schob. See Nauroth & Held, *MESSERSCHMITT Bf 109*, Stuttgart 1978.

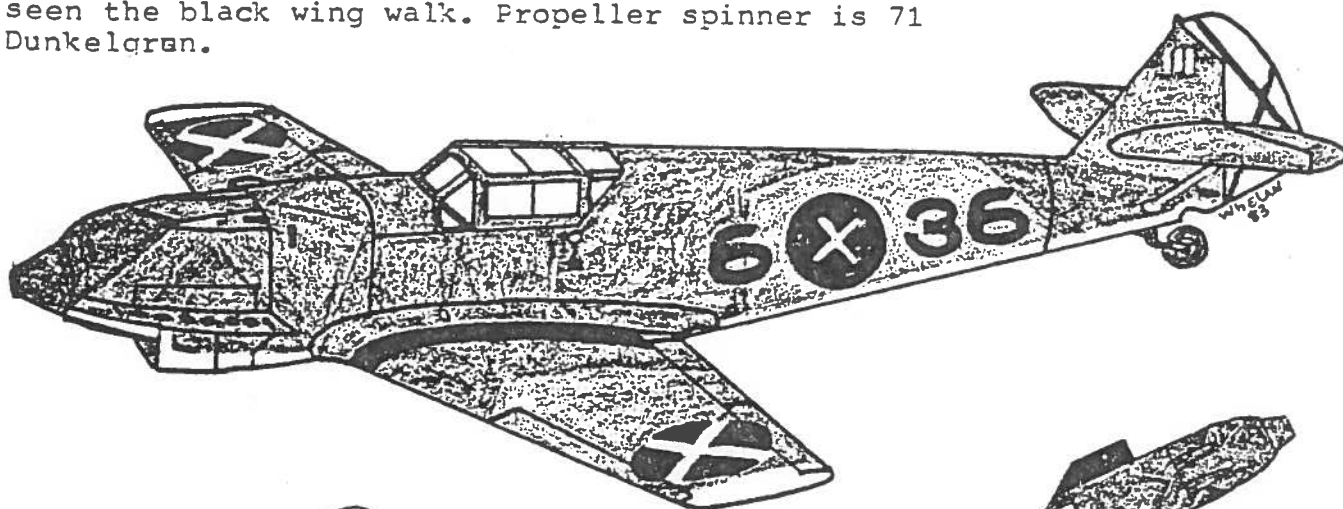
15. See Gil Mugarza, *ESPAÑA EN LLAMA 1936*, Barcelona 1968, p 479. Also, in September 1938, Obltn. Siebelt Reents took over command of 1.J/88 and the unit emblem was changed to the "Holzauge" or Wooden Eye.

16. There are a few published photos which are purported to show 6-51 & 6-47 in grey 63 schemes, but these are photos taken after the war as the new-style numbers demonstrate. See *Air Enthusiast Quarterly* #7, p 128 and Elstob, *LEGION CONDOR*, New York 1973, p 40. Also see A.E.Q. #7 p 194 for a good illustration of these post-war numbers.

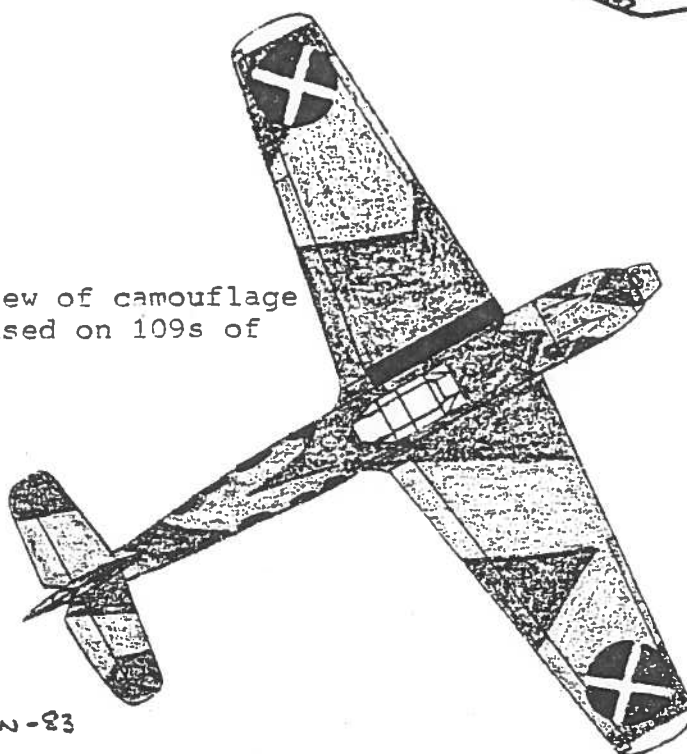
Copyright © 1983 Paul Whelan (SAFCH #553), 97 James St., Bloomfield, NJ 07003



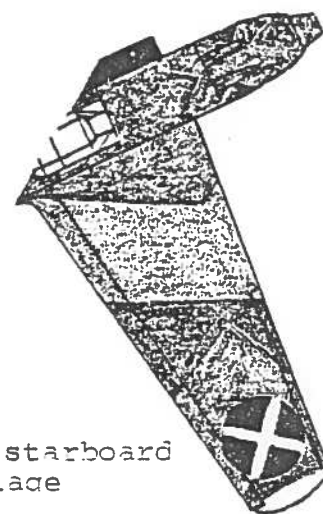
Bf 109B-2 as flown by Obltn. Harro Harder of 1.J/88 in August - Oct. 1937. On opposite side below can be seen the black wing walk. Propeller spinner is 71 Dunkelgrün.



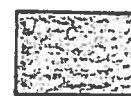
Upper view of camouflage scheme used on 109s of 1.J/88



Alternative starboard wing camouflage



70
Schwarzgrün

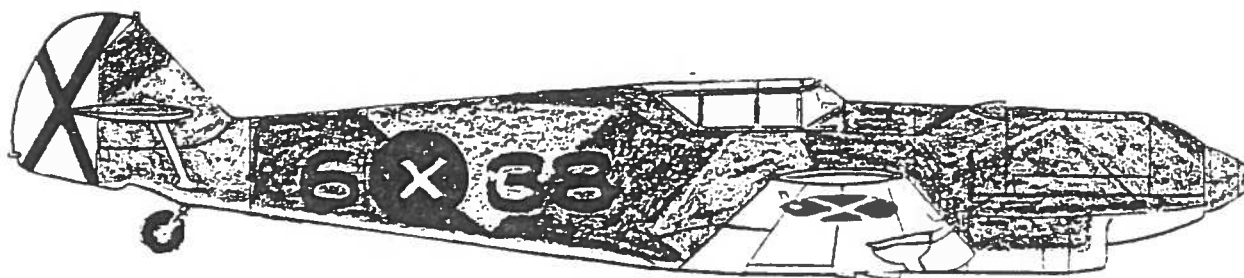


71
Dunkelgrün

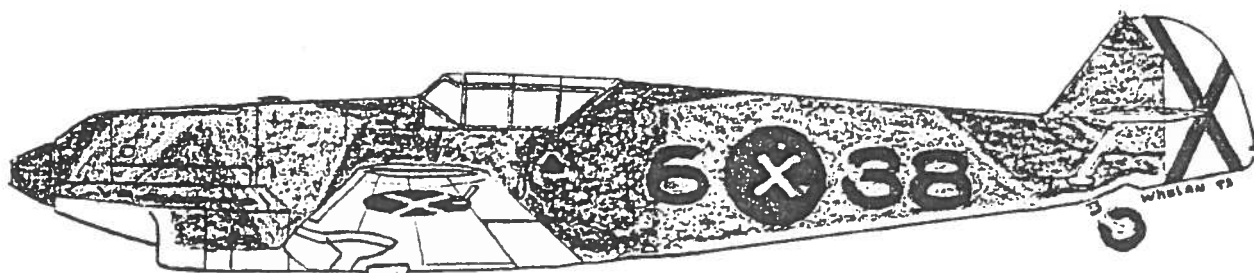


65
Hellblau

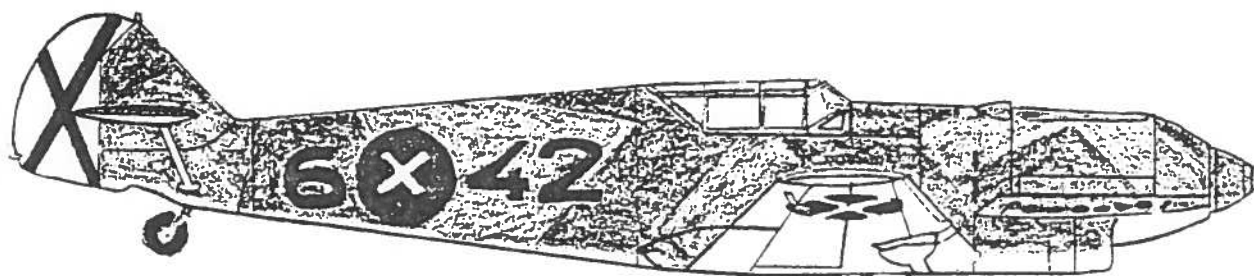
by Whelan-83



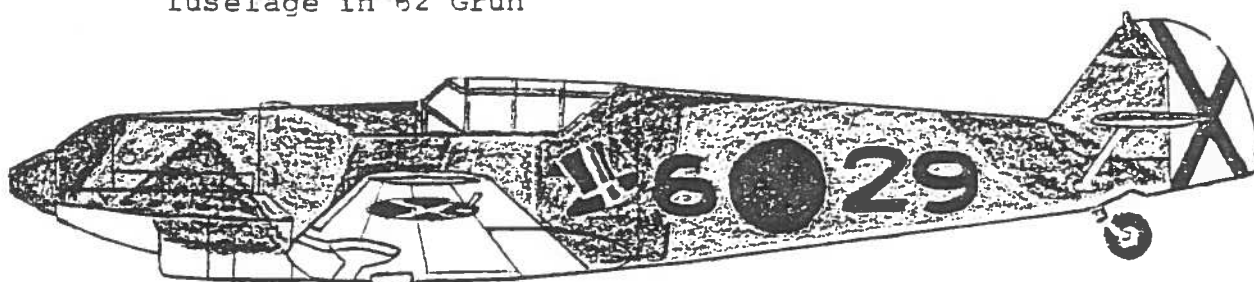
Bf 109B-2 flown by Uffz. Ernst Terry of 1.J/88 during August to October 1937



Opposite side of Terry's 109, note the white cross in the center of the black side disc this was the emblem of 1.J/88

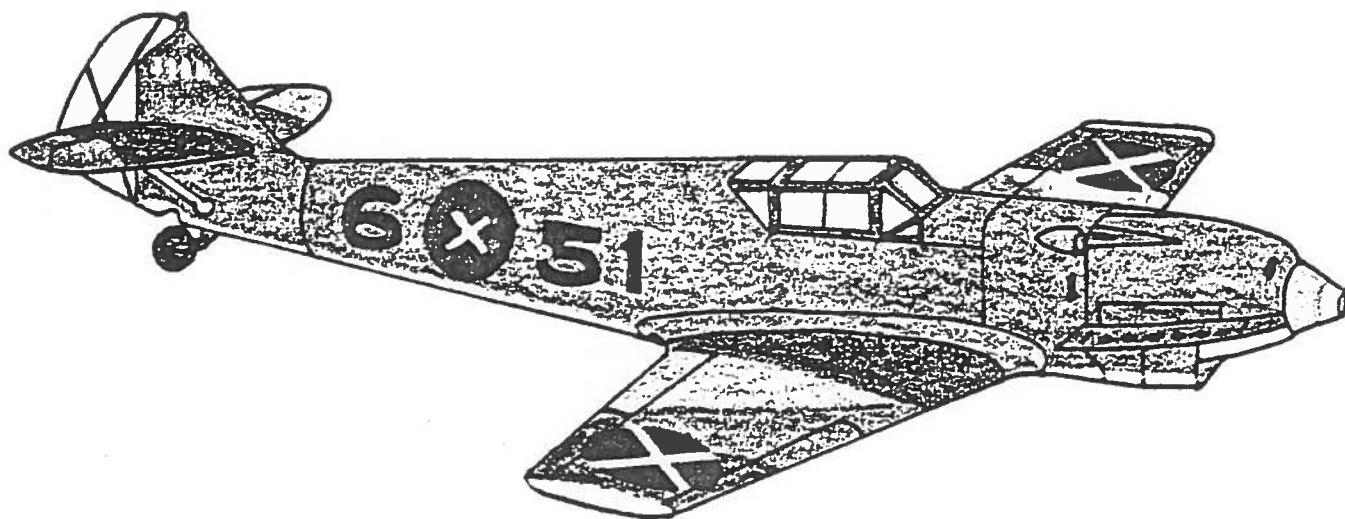


Bf 109B-2 of 1.J/88 this aircraft was later repainted with wings 61 Dunkelbraun, 62 Grün and 63 Hellgrau and fuselage in 62 Grün

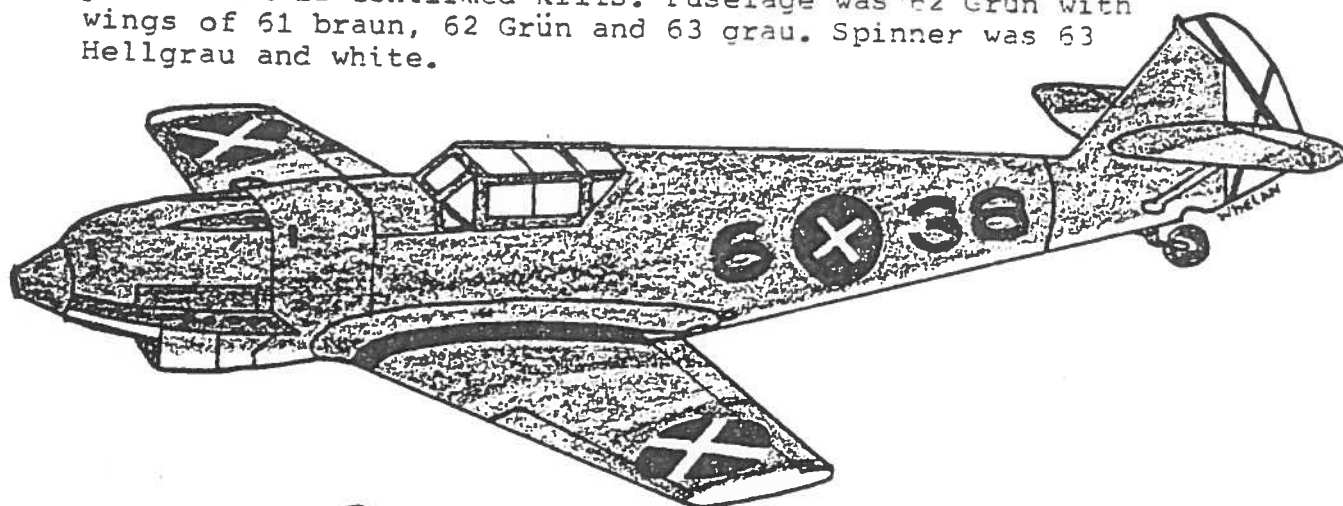


A B-2 of the second staffel, 2.J/88 shortly after arrival was repainted with all 63 Hellgrau upper and 65 blau lower surfaces

by Whelan-83

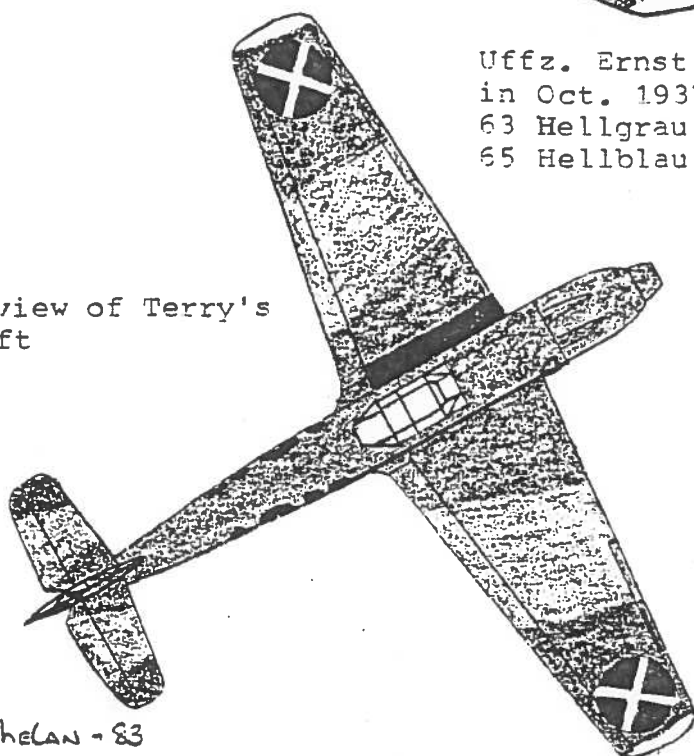


B-2 flown by Obltn. Wolfgang Schellmann, commander of 1.J/88 Dec. 1937. Second highest scoring Legion Condor pilot with 12 confirmed kills. Fuselage was 62 Grün with wings of 61 braun, 62 Grün and 63 grau. Spinner was 63 Hellgrau and white.

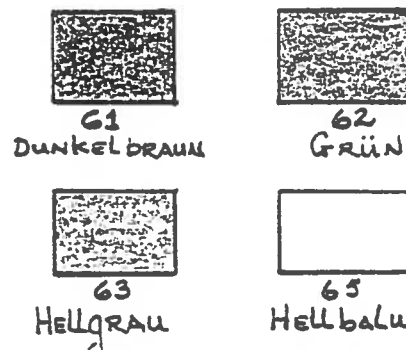


Uffz. Ernst Terry's 3-2 after repainting in Oct. 1937. Wings were in 62 Grün and 63 Hellgrau and fuselage in 62 Grün with 65 Hellblau lower

Upper view of Terry's aircraft



by Whelan - 83



CONFLICTS, COUPS, CRISES & CLASHES

A SURVEY OF THIRD-WORLD AIR COMBAT FROM 1946 TO THE PRESENT

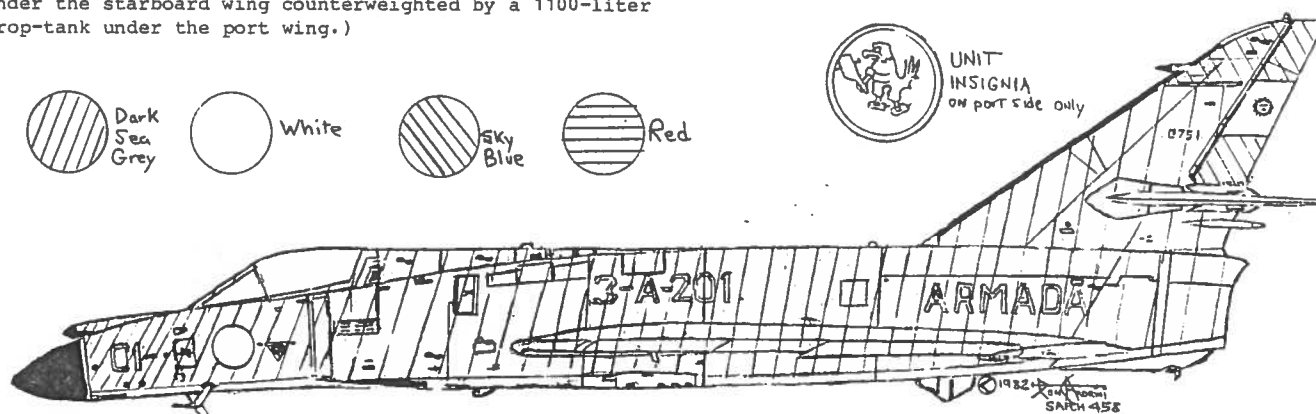
No. 28. Dassault-Breguet Super Etendard, 2 ° Escuadrilla de Caza y Ataque, 3 ° Escuadra Aeronaval Comando de Aviacion Naval Argentina; normally based at Comandante Espora but deployed to Rio Gallegos for the period of the conflict.

Much as a sudden storm in the South Atlantic some wars materialize seemingly from nowhere. Such was the case of the Falklands/Malvinas in April-June 1982. The short but intense bloodletting between Argentina and Britain is too recent to need recounting here.

At the time of the Argentine landings on the Falklands and South Georgia Island, it is believed that only five Super Etendards had been delivered to the ANA along with a like number of Am-39 Exocet air-to-surface missiles. The original Argentine plan was to operate the Super Etendard from the Venticinco de Mayo, but it seems that the carrier's catapults were incapable of launching a fully-loaded Super Etendard. (One Exocet is carried under the starboard wing counterweighted by a 1100-liter drop-tank under the port wing.)

After the war, in November 1982, France resumed deliveries when five more Super Etendards along with some Exocet missiles were embarked aboard an Argentine freighter. The original deal, contracted in 1979, called for the delivery of fourteen Super Etendards. Note the unit insignia most likely was painted over during the fighting. Colours for the insignia are as follows: red head, yellow beak, green body, white legs, black eyes, brown shoes & gloves, and dark grey club; white circle with red surround.

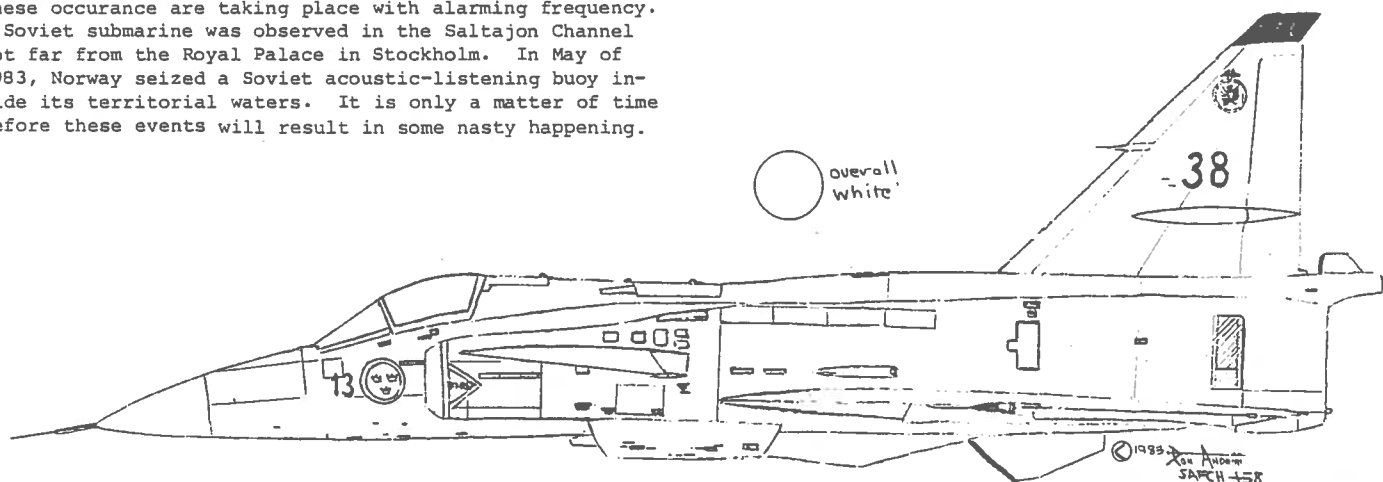
It is interesting to note that so far in this century Britain has partaken in two major conflicts in the Falklands region; in both the cornerstones of the British victory were ships named HMS Invincible. Historical deja vu?



No. 29. SAAB-Scania JA-37 Jakt-Viggen. Flottilj 13, Svensak Flygvapen; based at Norrköping/Bravalla, late 1982.

For many years, "unknown" submerged craft have been snooping along the shores of Sweden and neighboring states. At times, this snooping has taken place well within national boundaries. A classic case in point occurred on 30 October 1981 when a Soviet diesel-powered submarine ran aground off Karlskrona several miles inside Swedish territorial limits. The Soviets claimed the incident was caused by "navigational error". Since then these occurrences are taking place with alarming frequency. A Soviet submarine was observed in the Saltajon Channel not far from the Royal Palace in Stockholm. In May of 1983, Norway seized a Soviet acoustic-listening buoy inside its territorial waters. It is only a matter of time before these events will result in some nasty happening.

During some of these recent incidents, Swedish SH-37 surveillance-version Viggens have monitored and attempted to jam electronic transmissions from Tu-26 Backfire bombers to Soviet submarines. Late in 1982, an experimental overall white scheme was adopted for the JA-37s of F-13. So far, this has involved only six aircraft but, if warrented effective, it could be utilized on a broader scale.



No. 30. Israeli Aircraft Industries Nesher of the Chel Ha'Avir; based at Refidim (Bir Gafgafa) October 1973.

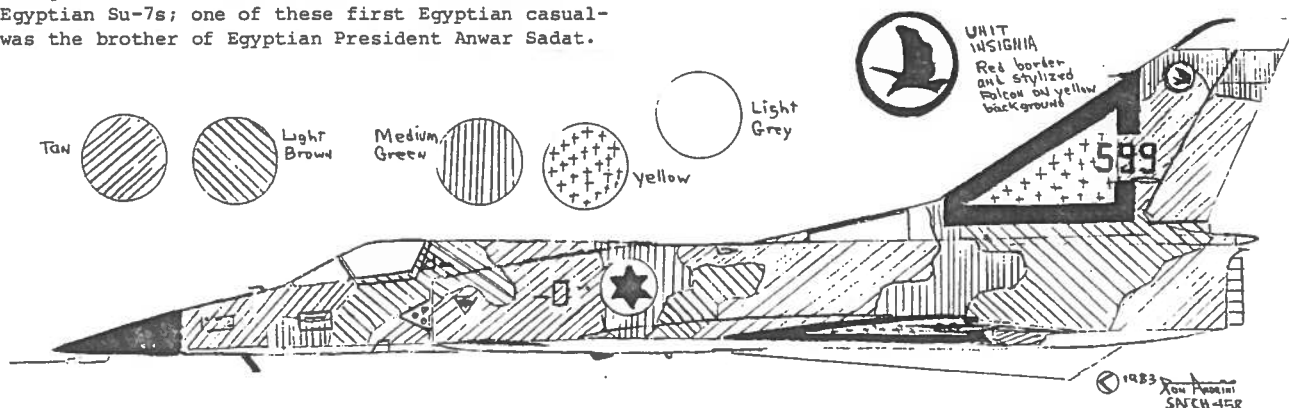
The Middle East can be compared to a cooking pot left over the fire too long. On 6 October 1973, Yom Kippur in Israel and the tenth day of Ramadan in the Arab nations, events boiled over for the fourth time since 1948. In the frantic first days of battle along the Golan Heights and the Sinai front, the Nesher played a vital role in Israeli air defense. At that time, only forty or so were in operation with the Israeli Defense Force Air Force.

Neshers of a unit based at Ramat-David downed two Syrians MiG-21s on the night of 5 October, less than 24 hours before the start of the Yom Kippur War. When hostilities erupted along the Sinai on 6 October, Neshers scrambling out of the forward base at Refidim downed several Egyptian Su-7s; one of these first Egyptian casualties was the brother of Egyptian President Anwar Sadat.

At first, the Neshers were deployed in shell-game fashion to provide local air superiority. However, when the Israeli situation improved, the Neshers flew combat missions on all fronts. Before the ceasefire went into effect on 24 October, some 450 Arab aircraft had been destroyed in air combat; more than one-fourth of the toll was attributed to the Nesher.

Replaced by the Kfir, some Neshers were later sold to Argentina. Renamed Dagger, they saw extensive action in the Falkland-Islands dispute with Britain in 1982.

Ron Andrini (SAFCH #458), 238 State St., San Mateo, CA 94401



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"New from Italy is a 1/72-scale vacuform kit of the SIAI Marchetti SF 260. This is a home-made product with a limited production run. The price is US \$10.00 (IMO or cash) including postage to the USA. Write to Mr. Giuseppe Penzani, Via Brescia 102/A, Cremona, ITALY.

"This kit is accurate in general dimensions and includes clear parts, three-view drawings, and good instructions. Wheels and prop are not included but you can use parts from the Airfix Cessna O2A or similar small aircraft. This kit was made to celebrate the Atlantic crossing by seven SF 260's to commemorate the 50th anniversary of the crossing of the Atlantic by 24 SM 55X's under the command of General Balboa.

"Mr. Penzani is now working on a vacuform kit of the Reggiane 2005 in 1/72 scale. As soon as it is available, I will let you know."

Silvio Poli (SAFCH #587), Via Callegari 10, 25121 Brescia, ITALY

THE RAND McNALLY ENCYCLOPEDIA OF MILITARY AIRCRAFT 1914-1980. Ed. by Enzo Angelucci. "A comprehensive source of info on all military aircraft worthy of mention, complete with technical data, national markings, photos and historical background. Incl. 1706 airplanes in color, 772 cutaway drawings, 252 photos, 7 color diagrams, 15 color charts, 2791 b&w drawings, 1904 color drawings, 4 giant folding inserts, 546 pages. Pub. at \$100.00. Only \$29.95 plus \$2.40 postage." Publishers Central Bureau, Dept. 217, 1 Champion Ave., Avenel, NJ 07001.

WISE OWL WORLDWIDE PUBLICATIONS, J.J. Daileida, 4314 West 238th St., Torrance, CA 90505.

SAFCH member Joe Daileida can provide subscriptions to SCALE MODELS for \$27.00 and PROPLINER for \$22.00. Also available are back issues of SCALE MODELS at \$3.50 each, PAM NEWS at \$2.75 each, and the first issue of PLASTIC KIT CONSTRUCTOR for \$2.25.

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VIRUS PLASTICUS (IPMS SWITZERLAND, Olivenstr. 2, 9320, Arborn; 4 issues for Sfr 24)

2/83 (32 pages) "de Havilland DH-112 Venom (7) Details Supplementaires" 5 pages. "Douglas DC-9 der Austrian Airlines" 3 pages including fleet list and 2 photos. "Die Morane Story (3)" 10 pages including 4 photos, 3 pages of side-view and plan-view drawings, and 1/48-scale 5-view drawing of the Swiss D-3801. This issue includes a one-page English summary.

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DIRTY PLASTIC (IPMS PHOENIX, 509 W. Camino Dr., Phoenix, AZ 85021; 4 issues for \$6.00.)

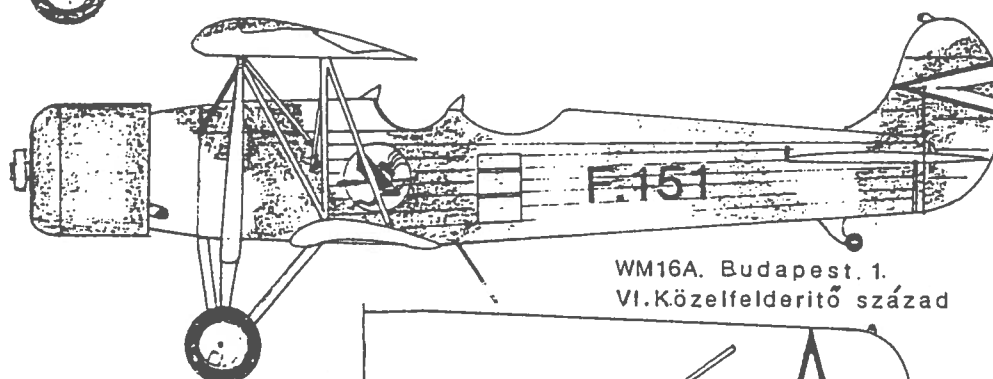
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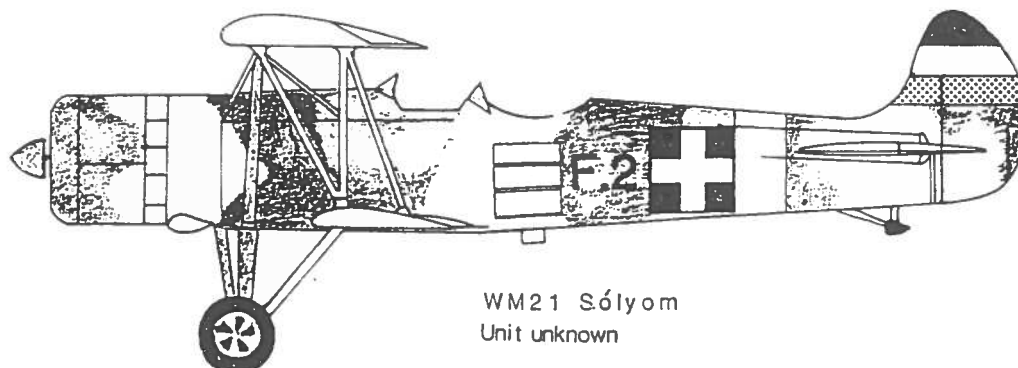
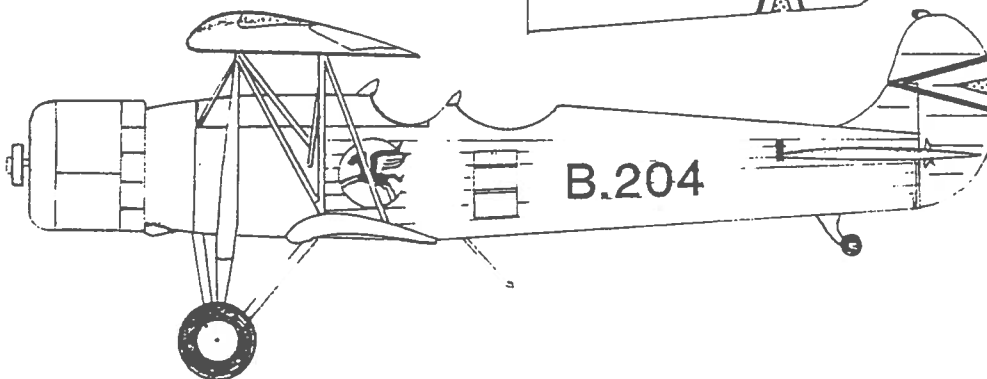
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